



# MORGANTOWN PLANNING COMMISSION

September 11, 2014  
6:30 PM  
City Council Chambers

**President:**

Peter DeMasters, 6<sup>th</sup> Ward

**Vice-President:**

Carol Pyles, 7<sup>th</sup> Ward

**Planning Commissioners:**

Sam Loretta, 1<sup>st</sup> Ward

Tim Stranko, 2<sup>nd</sup> Ward

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Bill Petros, 4<sup>th</sup> Ward

Mike Shuman, 5<sup>th</sup> Ward

Ken Martis, Admin.

Jennifer Selin, City Councilor

## **STAFF REPORT**

**CASE NO:** S14-07-III / CA Student Living / 494 Spruce Street

**REQUEST and LOCATION:**

Request by Lisa Mardis of Project Management Services, on behalf of CA Student Living, for approval of a Type III Site Plan – Development of Significant at 494 Spruce Street.

**TAX MAP NUMBER(s) and ZONING DESCRIPTION:**

Tax Map 26, Parcels 245 and 246; B-4, General Business District.

**SURROUNDING ZONING:**

South and West: B-4, General Business District

North and East: B-1, Neighborhood Business District

**BACKGROUND:**

The petitioner seeks to redevelop the VFW Post 548 site at the corner of Spruce Street and Willey Street by bringing a revised development program from that reviewed and denied by the Planning Commission on 12 JUN 2014. Addendum A of this report illustrates the location of the subject site.

**Proposed Development Program**

The following generally summarizes the proposed development program illustrated in the petitioner's application documents. The petitioner will highlight during their presentation how this program has been modified from the 12 JUN 2014 proposal.

- Twelve (12) stories with portions of the lowest three (3) stories below the adjoining grade.
- 89 "Mixed-Use Dwelling" units for a total of 331 bedrooms.
- 7,310 square feet of non-residential space on three (3) levels with approximately 4,042 square feet of retail space on the lowest level (Level P1) facing Spruce Street.
- 158 vehicle parking spaces on four (4) parking decks, one (1) truck loading space, and four (4) standard vehicle loading spaces. Only one point of ingress from Willey Street will be provided for all four (4) parking decks. Egress for all four (4) parking decks will be provided at two (2) locations; one on Willey Street and one on Spruce Street. Additionally, 97 bicycle storage spaces are proposed in one common facility on Level P2 with access from Willey Street.
- Above ground utility facilities along the development site's Spruce Street and Willey Street frontages are planned for relocation to below ground utility service trenches or vaults.

**Development Services**

Christopher Fletcher, AICP  
Director

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The petitioner presented the modified development to the Design Review Committee meeting on 12 AUG 2014.

**DSI Application Exhibits**

The following list identifies documents submitted by the petitioner as a part of the subject Development of Significant Impact Site Plan petition.

- Type III Development of Significant Impact Site Plan Application.
- Drawings prepared by Erdy McHenry Architecture, LLC dated 11 JUN 2014.
- Drawings prepared by Alpha Associates, Inc. dated 06 AUG 2014.
- Pedestrian Wind Flow Analysis Report prepared by ISOENV Environmental Design Lab dated 09 AUG 2014.
- Revised cover letter from Mark Metil, P.E., PTOE of Gannett Fleming dated 07 AUG 2014 addressing development proposal modifications in relation to the Traffic Impact Study dated 28 MAY 2014 prepared by Gannett Fleming.
- Letter from Gregory L. Bailey, P.E., West Virginia State Highway Engineer addressing West Virginia Division of Highway's review of development proposal modifications.
- A supplemental letter from Tom S. Witt, Ph.D. dated 03 SEP 2014 concerning his Economic Impact report dated 05 JUN 2014.

**Required Planning and Zoning Code Approvals**

The following Planning and Zoning Code related approvals are required for the development program as proposed. Each case number is followed with a brief description.

*Planning Commission*

Case No. S14-07-III .....Development of Significant Impact Site Plan.

Article 1385.05 provides that developments with 12 to 99 dwelling units are considered "Developments of Significant Impact" (DSI) which are those that have a neighborhood or citywide impact and involve the transportation network, environmental features such as parks or corridor streams, and local schools. DSI Site Plans are reviewed and approved by the Planning Commission.

The issue of whether or not the proposed development should be considered a Major Development of Significant Impact rather than a Development of Significant Impact was raised during the Planning Commission's 12 JUN 2014 hearing.

For clarification purposes, Article 1385.05 provides that "Mixed-Use Projects" of 100,000 or more square feet of gross floor area are to be considered Major Developments of Significant Impact. However, Article 1329.02 defines "Mixed-Use Development" as:

"A single development of more than one building and use, where the different types of land uses are in close proximity, planned as a unified complementary whole, and functionally integrated to the use of shared vehicular and pedestrian access and parking area, but not a mixed-use dwelling as defined in this Ordinance."

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By definition, the correct site plan grouping for the subject project is Development of Significant Impact.

Case No. MNS14-05.....Minor Subdivision.

The development site includes Parcels 245 and 246 of Tax Map 26, which must be combined so that the proposed structure is situated on one (1) parcel rather than crossing two (2) parcels.

*Board of Zoning Appeals*

Case No. V14-24 .....Variance relief as it relates to minimizing canyon effects.

Article 1351.01(I) provides that buildings taller than three (3) stories shall incorporate design elements that preserve adequate light and airflow to public spaces including streets and sidewalks. A variance petition has been submitted so that the BZA can determine whether or not proposed design elements minimize canyon effects as required. If the BZA agrees that said elements further desired mitigation design techniques, than it can rule accordingly. If the BZA does not agree that said elements meet desired mitigation design techniques, than it can determine whether or not to grant related variance relief. This variance petition is scheduled for consideration by the BZA's at its 17 SEP 2014 hearing.

**ANALYSIS:**

Comprehensive Plan and Downtown Strategic Plan

As recommended in Chapter 9 "Implementation" of the 2013 Comprehensive Plan Update, Addendum B of this report identifies how the proposed development program relates to the land management intent, location, and pattern and character principles of the current Comprehensive Plan.

Additionally, Addendum B includes sections of the 2010 Downtown Strategic Plan Update.

It should be noted that "shall" statements within the Comprehensive Plan or the Downtown Strategic Plan must be understood as desired objectives and strategies that do not have the force or effect of law unless incorporated into the City's Planning and Zoning Code.

Staff encourages the Planning Commission to review both Plans for guidance as Addendum B is not intended to represent a complete comparative assessment.

It is the opinion of the Planning Division that the modified development program has improved the project's concurrence with the 2013 Comprehensive Plan Update.

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## Site Location

The VFW Post 548 site represents a unique opportunity to strategically locate higher residential density in close proximity to West Virginia University's downtown campus. Because the site is well served by public transit and is within walking and biking distance of primary destinations, residents can access alternate modes of transportation thereby reducing auto dependency and mitigating increased traffic congestion created by commuting traffic from higher density residential development outside the City of Morgantown.

To this point, Land Management Objective LM 5.2 of the 2013 Comprehensive Plan Update provides, "Permit higher density development in areas that are well-supported by existing or planned transportation infrastructure or transit services."

## Residential Density

As Addendum B of this report identifies, the desired residential development pattern within the downtown is provided in the following Comprehensive Plan objectives and guidelines.

- LM 1.5 Create incentives for developers to build residential units downtown that serve a broad age and socioeconomic range.
- NH 4.1 Provide incentives to developers to encourage development of alternative housing types (i.e. higher density, live-work, mixed-use) in designated growth areas.

Additionally, the following objectives and guidelines are providing in the Downtown Strategic Plan for Character Area C4 – Forest Avenue.

- Reinforce the urban quality by increasing the mass, density, and mixed-use quality buildings that front on well-designed pedestrian streets.
- Maximize residential opportunities to take advantage of the location near WVU campus.
- Create balance and harmony in the vertical and horizontal massing of buildings.



- Buildings should be oriented toward streets and open spaces along an established "build-to-line" so that an urban edge is created with the buildings.
- Buildings should exhibit continuity in the design of their facades.
- Buildings that front streets and open spaces should have a well-design and scaled first floor with human scaled elements, doors, windows, awnings, and stoops.
- Increase the supply, diversity, range, and affordability of housing opportunities within the downtown.
- 24/7/365 living, activity, commerce, and energy will create a safer downtown.

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The petitioner's proposed bedroom composition has been modified to include two-, three-, and four-bedroom units along with four-bedroom loft style units. It appears that the bedroom composition modification from the previous program supports the desired objective to diversify the housing stock within the immediate area given nearby existing and under-construction residential offerings.

The maximum residential density standard provided in Article 1349.07 is based on the number of dwelling units in relation to the area of the development site. Bedroom composition of the proposed dwelling units or the number of proposed occupants is not measured in the maximum residential density standard. This is a land use policy challenge for any college/university community with a percentage of student-oriented housing stock that functions and performs very differently than conventional higher density housing occupied by more diverse and less transient households.

### **Traffic Impact Analysis**

Article 1385.08(A)(1)(g) of the Planning and Zoning Code provides that site plan applications for Developments of Significant Impact must be accompanied by an approved West Virginia Division of Highways (WVDOH) Access Permit, if applicable. Because Spruce Street and Willey Street are both a part of the WVDOH's roadway system, this application element is required.

The petitioner's Traffic Impact Analysis dated 28 MAY 2014 and a revised cover letter dated 07 AUG 2014 from Mark Metil, P.E., PTOE of Gannett Flemming addressing development proposal modifications was submitted to WVDOH and the City Engineer for review and comment. WVDOH completed its review as evidenced by the letter from Gregory L. Bailey, P.E., West Virginia State Highway Engineer.

It should be noted that the City and WVDOH follow a collaborative review and comment process for traffic impact studies so that both authorities' concerns are addressed prior to the issuance of access permits. Enforcing a literal application of Article 1385.08(A)(1)(g) will complicate this collaborative review process in terms of ensuring City Administration and Planning Commission capability to fully participate in and influence related decision-making.

It is the opinion of the Planning Division that the Planning Commission should proceed in its review of the present DSI site plan petition relative to WVDOH access permitting by including a condition, should the Planning Commission approve the present DSI site plan petition, that requires WVDOH access permit approval; provided there are no changes in the locations and/or designs, as a result of WVDOH access permit approval, of the proposed driveway entrances illustrated on the plans presented herein.

### **Parking**

The petitioner's development program modification addresses concerns raised during the 12 JUN 2014 hearing over a lack of sufficient onsite and nearby public parking to adequately serve the demand for parking generated by the development. Specifically, the modified development plan eliminates all variances and/or conditional use approvals associated with on-site parking reductions and meeting minimum on-site parking requirements. The addition of the below-grade parking deck and reconfiguration of the

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upper three parking decks provides 158 parking spaces, which exceeds the minimum parking requirement for the development.

### **Loading**

The modified development program includes one (1) local pick-up and delivery truck loading space to serve nonresidential delivery needs and four (4) standard-sized vehicle loading spaces to serve residential delivery needs. The provision of loading spaces meets related loading requirements, which addresses concerns raised during the 12 JUN 2014 hearing.

### **Outdoor Space**

The modified development program eliminates the pool/hot tub facility from the upper floor outdoor recreation space as illustrated below. Additionally, physical barriers have been designed to provide a buffer between the outdoor space and the edge of the building closest to the adjoining child development center. This modification appears to address concerns raised during the 12 JUN 2014 hearing.



### **Building Height**

Section 6.3.1.4 of the 2010 Downtown Strategic Plan Update recommends that new buildings within the Character Area C4 – Forest Avenue “shall be” a maximum height of four (4) stories or 50 feet or a minimum of three (3) stories or 30 feet in height to promote a mix of uses and a continues urban edge.

As noted above, “shall” statements within the Comprehensive Plan or the Downtown Strategic Plan must be understood as desired objectives and strategies. These statements should be utilized solely as guidelines. In the context of the Comp Plan or the Downtown Plan, the term “shall” is a writing style that can lead to confusion.

The noted minimum and maximum building height strategy is a guideline and not a mandated standard or regulation. The subject building height guideline has not been codified into the City’s Planning and Zoning Code and therefore has no force or effect of law. A decision to deny a development based on a guideline enumerated in the Comprehensive Plan or the Downtown Strategic Plan that has not been codified in the City’s Planning and Zoning Code could be argued as arbitrary and capricious.

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### **STAFF RECOMMENDATION:**

Staff recommends approval of Case No. S14-07-III with the following conditions:

1. That minor subdivision petition Case No. MNS14-05 combining Parcels 245 and 246 of Tax Map 26 must be approved and final plat recorded prior to issuance of the certificate of occupancy.
2. That Variance Case No. V14-24 must be approved by the Board of Zoning Appeals (BZA) and all related conditions therein observed and/or addressed accordingly.
3. That access permitting from the West Virginia Division of Highways must be obtained; provided, said approval does not alter the arrangement of driveway locations and/or designs illustrated on the site plans reviewed and approved herein. Should access permitting alter the arrangement of said driveway locations and/or designs, than Planning Commission review and approval must be obtained prior to the issuance of any building permit for the site.
4. That the developer shall continue to consult with the Downtown Design Review Committee and accordingly address the Committee's comments and concerns where practicable.
5. That the final Landscape Plan and Erosion Control Plan be submitted with the building permit application for review and approval. Variance approval must be obtained should said plans not conform to the related performance standards set forth in the City's Planning and Zoning Code.
6. That the specific land use for the 4,042 square feet retail/commercial/office space at-grade with Spruce Street must conform to Table 1331.05.01 "Permitted Land Uses" and supplemental regulations thereto.
7. That a master Signage Plan must be submitted and reviewed under the standard building permit application process once nonresidential uses are identified. Variance approval must be obtained should the master Signage Plan not conform to related performance standards set forth in the City's Planning and Zoning Code.
8. That the development must meet all applicable federal Fair Housing and Americans with Disabilities Act standards as determined by the City's Chief Building Code Official.
9. That public sidewalks along the development site's Spruce Street and Willey Street frontages shall be reconstructed to the satisfaction of the City Engineer and, where practicable, incorporate design elements utilized in the High Street streetscape improvement projects and the planned Walnut Street streetscape improvement project.

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10. That, as proposed by the petitioner, above ground utility facilities along the development site's Spruce Street and Willey Street frontages must be relocated underground; provided all affected utilities, the West Virginia Division of Highways, and the City Engineer approve development plans for same.
11. That the developer shall consult with the City Engineer in providing public trash receptacle(s) and bench(es) near retail entrance(s) that match existing facilities within the downtown; provided said street furniture does not reduce the width or obstruct public sidewalks.

Attachments: As noted above.

**Development Services**

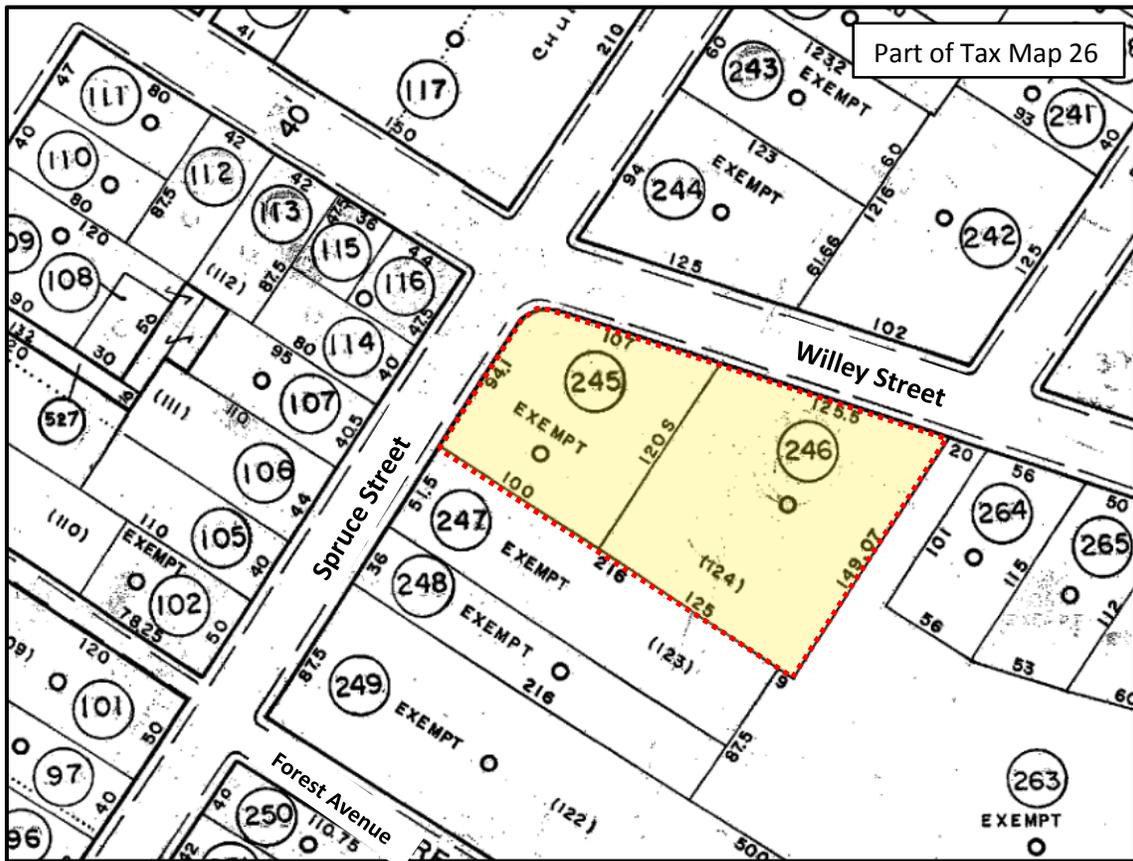
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**STAFF REPORT ADDENDUM A**  
**S14-07-III / CA Student Living / 494 Spruce Street**





## STAFF REPORT ADDENDUM B

### S14-07-III / CA Student Living / 494 Spruce Street

#### Concurrence with the 2013 Comprehensive Plan Update

The following narrative identifies where, in the opinion of the Planning Division, the subject development of significant impact is in concurrence and/or is inconsistent with the 2013 Comprehensive Plan Update.

<b>INTENT</b>	Development proposals will reflect the spirit and values expressed in the Plan's principals.
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#### Principles for Land Management

Principal 1	Infill development and redevelopment of underutilized and/or deteriorating sites takes priority over development in green field locations at the city's edge.	<input checked="" type="checkbox"/> Concurrence <input type="checkbox"/> Inconsistent <input type="checkbox"/> Other
	<i>The VFW Post 548 site is located within the "Encouraged Growth" area, the "Core" pattern and character area, and the "Downtown Enhancement" area and is not located within a green field location at the city's edge.</i>	
Principal 2	Expansion of the urban area will occur in a contiguous pattern that favors areas already served by existing infrastructure.	<input checked="" type="checkbox"/> Concurrence <input type="checkbox"/> Inconsistent <input type="checkbox"/> Other
	<i>The VFW Post 548 site is located within the central urban core and appears to be supported by existing multi-modal transportation options and adequate utility infrastructure capacity.</i>	
Principal 3	Downtown, adjacent neighborhoods and the riverfront will be the primary focus for revitalizations efforts.	<input checked="" type="checkbox"/> Concurrence <input type="checkbox"/> Inconsistent <input type="checkbox"/> Other
	<i>The VFW Post 548 site is located within the B-4 District and appears to leverage its proximity with the University's downtown campus, which should further desired strengthening of the city's urban core in terms of walkability, customer-base, and proximity to residents' primary destinations.</i>	
Principal 4	Existing neighborhoods throughout the city will be maintained and/or enhanced.	<input checked="" type="checkbox"/> Concurrence <input type="checkbox"/> Inconsistent <input type="checkbox"/> Other
	<i>The VFW Post 548 site is not located within or adjacent to a "Neighborhood Conservation" area.</i>	

Principal 5	Quality design is emphasized for all uses to create an attractive, distinctive public and private realm and promote positive perceptions of the region.	<input checked="" type="checkbox"/> Concurrence <input type="checkbox"/> Inconsistent <input type="checkbox"/> Other
<p><i>The developer's professional design team has consulted with the Downtown Design Review Committee (DRC) and incorporated modifications that appear to address the Committee's comments and concerns in terms of architectural style, cladding material and color, window rhythm, upper floor setback, increased at-grade setback, etc. The DRC noted during its 12 AUG 2014 that significant improvements were made from the proposal reviewed by the Planning Commission on 12 JUN 2014.</i></p>		
Principal 6	Development that integrates mixed-uses (residential, commercial, institutional, civic, etc.) and connects with the existing urban fabric is encouraged.	<input checked="" type="checkbox"/> Concurrence <input type="checkbox"/> Inconsistent <input type="checkbox"/> Other
<p><i>The proposed development includes residential and street-level nonresidential uses. The urban fabric within the immediate built environment is heterogeneous given the various development pattern and character types, scales and densities, forms and functions, and construction periods.</i></p>		
Principal 7	Places will be better connected to improve the function of the street network and create more opportunities to walk, bike and access public transportation throughout the region.	<input checked="" type="checkbox"/> Concurrence <input type="checkbox"/> Inconsistent <input type="checkbox"/> Other
<p><i>The VFW Post 548 site is well served by public transit along Spruce Street and Willey Street and is within walking and biking distance of the University campus, downtown PRT station, the downtown central business district. Redevelopment of the site to a higher residential density links residents to alternate modes of transportation thereby reducing auto dependency within the City and mitigating increased traffic congestion created by commuting traffic from outside the City. The proposed at-grade setbacks appear to functionally widen adjoining public sidewalks. Significant bicycle storage is provided.</i></p>		
Principal 8	A broad range of housing types, price levels and occupancy types will provide desirable living options for a diverse population.	<input checked="" type="checkbox"/> Concurrence <input type="checkbox"/> Inconsistent <input type="checkbox"/> Other
<p><i>The proposed development program increases housing choice and diversity in the context of the immediate residential area, which includes the Courtyard East and Courtyard West multi-family developments (four and five stories respectively); the ten-story, multi-family high-rise, age-restricted Unity Manor building; the six-story Central Place multi-family development under construction; various duplex and triplex configurations; and, converted single-family residential units. The petitioner's modified development program includes two-, three-, and four-bedroom units along with four-bedroom loft style units. Given the infancy of the 2013 Comprehensive Plan adoption, zoning ordinance dictates and/or guidelines concerning desired tenancy, affordability, and workforce opportunities have not been developed or enacted.</i></p>		

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Principal 9 Residential development will support the formation of complete neighborhoods with diverse housing, pedestrian-scaled complete streets, integrated public spaces, connection to adjacent neighborhoods, and access to transportation alternative and basic retail needs.  Concurrency  
 Inconsistent  
 Other

*The VFW Post 548 site is within the B-4, General Business District and located within a two to five minute relatively flat walk to basic retail goods and services, civic, institutional, and public spaces located within the central downtown business district and University's downtown campus. Semi-public indoor and outdoor spaces have been incorporated to further quality of life, convenience, and enjoyment of the development's residents. Special design care appears to have been given to further the at-grade pedestrian level experience and including transparency, materiality, and orientation.*

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Principal 10 Parks, open space, and recreational areas are incorporated as part of future development.  Concurrency  
 Inconsistent  
 Other

*Semi-public indoor and outdoor spaces have been incorporated to further quality of life, convenience, and enjoyment of the development's residents. The proposed at-grade setbacks appear to functionally widen adjoining public sidewalks. Green-wall landscaping treatments along Willey Street and on the side wall adjoining the child development center should serve to soften the relationship between the public sidewalk realm and the building's at-grade edge. Additionally, the pool/hot tub facility has been removed from the above-grade outdoor recreation space and physical barriers are planned to buffer said space from the building edge.*

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Principal 11 Environmentally sensitive and sustainable practices will be encouraged in future developments.  Concurrency  
 Inconsistent  
 Other

*Stormwater management best practices will be required for a large site currently lacking such measures. The developer's goals and objectives concerning sustainable construction techniques and industry accepted best practices have not been fully developed.*

**LOCATION**

Development proposals will be consistent with the Land Management Map. If the proposal applies to an area intended for growth, infill, revitalization, or redevelopment, then it should be compatible with that intent and with any specific expectations within Areas of Opportunity. If the proposal applies to an area of conservation or preservation, it should be compatible with and work to enhance the existing character of the immediate surroundings.

The following graphic is clipped from the **Conceptual Growth Framework Map** included on Page 19 of the 2013 Comprehensive Plan Update. The subject development site is located within the “**Encouraged Growth**” area.



The following graphic is clipped from **Map 3 – Pattern and Character** included on Page 27 of the 2013 Comprehensive Plan Update. The subject development site is located within the “**Core**” pattern and character area.



**Core.** The Core is the zone of densest development and is generally defined as Downtown Morgantown. The area has the highest level of connectivity with a grid street pattern with short walkable block lengths. Buildings range from two to twelve stories and are located close to each other and to the street. A mixed-use district, the core contains a range of retail, office, institutional and residential activities, with many buildings containing multiple uses within them. The street, network, building density and mix of uses support a high degree of pedestrian mobility.



The following graphic is clipped from **Map 4 – Land Management** included on Page 39 of the the 2013 Comprehensive Plan Update. The subject development site is located within the “**Downtown Enhancement**” concept area.



- Downtown Enhancement:** Continued infill and redevelopment in the Downtown core with a mix of employment, civic, commercial and residential uses as described in the 2010 Downtown Strategic Plan Update.

**PATTERN AND CHARACTER**

Development proposals in growth areas will be consistent with preferred development types. Development in areas where growth is not intended should be compatible with the relevant Character Areas description and expectations for how those areas should evolve in the future.

The following graphics are clipped from Pages 41 through 43 of the 2013 Comprehensive Plan Update and identify the development types desired within the “Core Enhancement” concept area.

Appropriate Development Types

CONCEPT AREA	SF	TF	MF	C	NX	UC	CC	O	I	CD	OS
 Core Enhancement			•	•	•	•					•

**MF Multi-family Residential**

Includes various forms such as apartment buildings where three or more separate residential dwelling units are contained with a structure and townhouse dwelling types. They vary considerably in form and density depending on the context – from four-story or larger buildings set close to the street in and at the edge of the downtown core and along major corridors, to smaller two- to four-story buildings with greater street setbacks in areas between the downtown core and single-family neighborhoods.



**C Civic and Institutional**

These sites include both public uses (government buildings, libraries, community recreation centers, police and fire stations, and schools) and semi-public or private uses (universities, churches, hospital campuses). Public uses should be strategically located and integrated with surrounding development. Civic and Institutional sites may be distinctive from surrounding buildings in their architecture or relationship to the street.



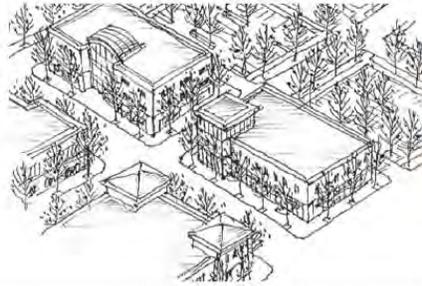
**NX Neighborhood Center Mixed-Use**

A mix of housing, office, commercial, and civic uses adjacent to one another or contained within the same structure (such as offices or apartments above ground-floor retail). Such uses should be compatible with and primarily serve nearby neighborhoods (within 1/2 mile). Parking should be located behind or to the side of buildings and may be shared between multiple uses.



**UC Urban Center Mixed-Use**

A mix of housing, office, commercial, and civic uses located adjacent to one another or sharing the same building. Buildings are generally larger in scale than neighborhood mixed-use and contain more employment and commercial uses that serve the broader community. Buildings should be located near the street with parking provided on-street or in shared parking configurations behind or between buildings.



**OS Greenspace**

Includes formal parks, recreation areas, trails, and natural open space.



**OBJECTIVES  
AND  
STRATEGIES**

**Land Management**

**A. Goal**

Efficient and attractive use of land resources that strengthens the quality, character, and upkeep of the built environment while balancing redevelopment and strategic expansion with open space preservation.

**Objective 1. Strengthen Downtown.**

- ➔ LM 1.5 Create incentives for developers to build residential units downtown that will serve a broad age and socioeconomic range.

**Objective 5. Encourage land use patterns that support improved transportation choice and efficiency.**

- ➔ LM 5.2 Permit higher density development in areas that are well-supported by existing or planned transportation infrastructure or transit services.

**Objective 6. Improve community appearance, particularly at city gateways.**

- ➔ LM 6.5 Encourage major redevelopment projects to relocate utilities from view of primary corridors, arterials, and collectors with emphasis on underground placement.

**OBJECTIVES  
AND  
STRATEGIES**

**Neighborhoods and Housing**

**A. Goal**

Attractive, well-maintained neighborhoods that offer a broad mix of desirable housing options and convenient access to services and amenities.

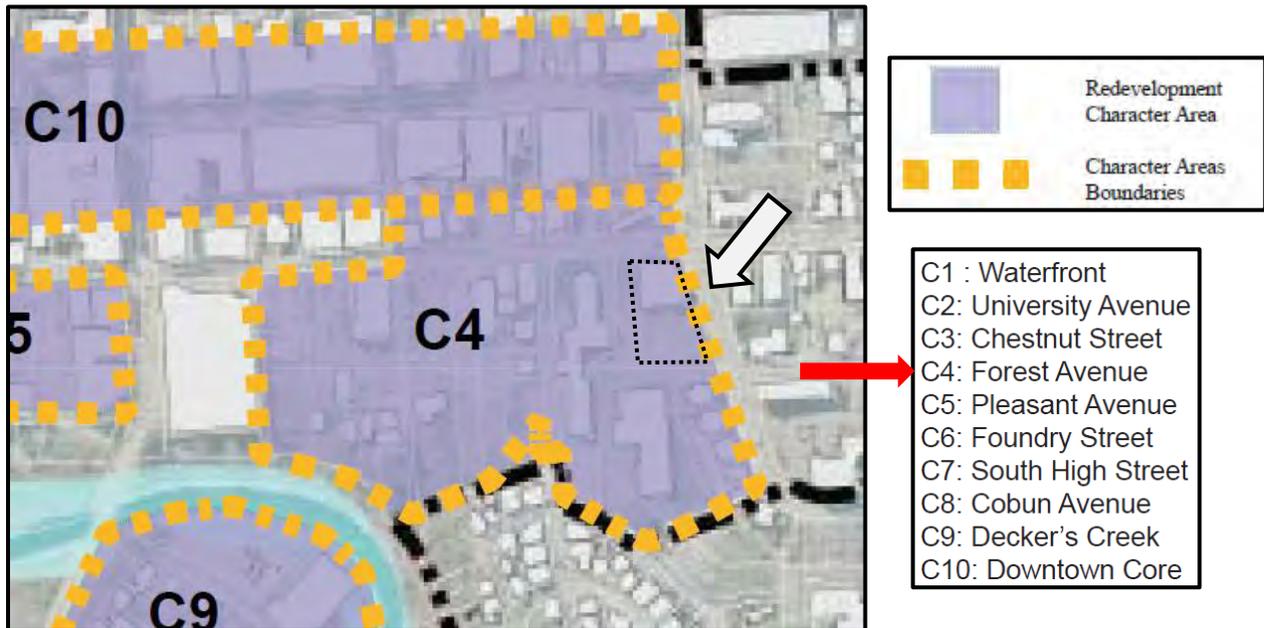
*Objective 4. Promote the development of a broad range of housing types and prices.*

➔ NH 4.1 Provide incentives to developers to encourage development of alternative housing types (i.e. higher density, live-work, mixed-use) in designated growth areas.

**2010 Downtown Strategic Plan**

**Concurrence with the 2010 Downtown Strategic Plan**

The following graphics have been clipped from the 2010 Downtown Strategic Plan.



Clipped from Page 89

## 6.0 Downtown Strategies

### 6.3.1.4 Character Area C4 – Forest Avenue

<p><b>STRENGTHS</b></p> <ul style="list-style-type: none"> <li>• Farmer's Market.</li> <li>• Immediately adjacent to WVU campus.</li> <li>• Proximity to High Street retail.</li> <li>• Gateway and downtown access point both to and from Woodburn residential neighborhood.</li> <li>• Various historic architecture, churches, and homes provide scale and interest.</li> <li>• Access to Whitmore Park and trails with access to downtown and Sabraton.</li> <li>• Plans are currently underway for the redevelopment of the old Central School site.</li> </ul>	<p><b>CHALLENGES</b></p> <ul style="list-style-type: none"> <li>• Some areas of steep topography.</li> <li>• Poorly designed, planned and managed student housing.</li> <li>• Minimal lighting and sidewalks are in disrepair.</li> <li>• Underutilized existing properties and original street grid has been disrupted in some places.</li> <li>• Insufficient supply of off-street parking.</li> </ul>
<p><b>OPPORTUNITIES</b></p> <ul style="list-style-type: none"> <li>• New mixed-use student housing village with live-work units for young professionals.</li> <li>• Additional mixed-use infill at the north end of Spruce Street.</li> <li>• Entertainment or cultural facilities.</li> <li>• Direct access to Deckers Creek and Deckers Creek Trail.</li> <li>• A cohesive sense of place in this area through urban design streetscapes, signage, lighting, art and landscaping.</li> <li>• Enhance the setting for the Farmer's Market.</li> <li>• Promote the redevelopment of derelict student housing into new attractive student housing near campus.</li> </ul>	

### VISION / DEVELOPMENT THEME

A neighborhood with mixed-use live-work opportunities interspersed throughout, that is directly adjacent to downtown and the Farmer's Market. This area will also incorporate townhouses along Deckers Creek and some high quality student/young professional housing sprinkled throughout the area.

### ACTIONS

- 6.3.1.4a Create a more permanent structure for the Farmer's Market while still retaining the site's principal parking use (i.e. covered parking stalls); study the feasibility of allowing evening parking for performances at the Metropolitan Theatre.



## 6.0 Downtown Strategies

- 6.3.1.4b Offer incentives to enable consolidation of parcels and consistency in development theme and pattern. These lot consolidation incentives options are:
- Offer an increased floor area ratio (FAR) bonus to properties requesting lot consolidation. The bonus incentives would apply to the gross square footage of a single parcel following consolidation.
  - Offer residential density bonus incentives. The density bonus incentives would apply to the gross square footage of a single parcel following consolidation.
  - At the discretion of the city, fee assistance and other financial incentives could be made available to encourage lot consolidation activities, subject to available resources. Financial incentives may include, but not limited to:
    - i. Permit fee assistance (waivers, reduced fees, etc.)
    - ii. Reductions in approval procedure timeline.
    - iii. Others as deemed appropriate by the Planning Commission
- 6.3.1.4c Create more green space around new residential development in order to manage and clean stormwater before it enters Deckers Creek and travels into the Monongahela River.
- 6.3.1.4d Expand the government service needs of Morgantown into this area, which is adjacent to the existing government core.
- 6.3.1.4e Adopt and enforce Main Street Morgantown Urban Design Guidelines and Design Guidelines for Public Projects.
- 6.3.1.4f Create specific design guidelines for the "Forest Avenue Character Area".

### DESIGN GUIDELINES CONSIDERATIONS

#### General Intent / Goals

Dense pedestrian friendly village with buildings that are organized on an urban street pattern and along open space connections to Deckers Creek.

#### Planning Requirements

- ➔ Reinforce the urban quality by increasing the mass, density, and mixed-use quality buildings that front on well designed pedestrian streets.
- ➔ Maximize residential opportunities to take advantage of the location near WVU campus.
  - Create north-south pedestrian and bicycle access to the River at regular intervals at the ends of the alleys that extend to downtown.
- ➔ Create balance and harmony in the vertical and horizontal massing of buildings.
  - Create a consistent architectural style and palette of materials.
- ➔ Areas characterized as "New Mixed-Use Development" in Figure 17 will offer retail/commercial on the ground floor and either office or residential on the upper floors.

#### Building Height

- ➔ New buildings shall be a maximum height of four (4) stories or 50' or a minimum of three (3) stories or 30' in height to promote a mix of uses and a continuous urban edge.

*This is a guideline that does not have the force or effect of law unless codified in the City's Planning and Zoning Code.*



## 6.0 Downtown Strategies

### Setbacks

As described in B-4 zoning district.

### Parking and Access

As described in the B-4 standards with the addition of the City offering an option for reduced required parking amounts for downtown residential developers as described under Transportation Section 6.4.2.

### Building Placement

- ➔ • Buildings should be oriented toward streets and open spaces along an established “build to line” so that an urban edge is created with the buildings.
- ➔ • Buildings should exhibit continuity in the design of their facades.
- ➔ • Buildings that front streets and open spaces should have a well designed and scaled first floor with human scaled elements, doors, windows, awnings, and stoops.
- Buildings should consider pedestrian scaled rhythms along the street and open space network and provide architectural breaks or interest every 30 - 50 feet of horizontal distance.



*Inspirational imagery for Action 6.3.1.4c taken from Bloomington, Indiana depicting a pocket park located between two condominium buildings.*



## 6.0 Downtown Strategies

### Materials

Materials should conform to existing B-4 standards and be consistent with the materials chosen for the existing historic buildings within the “Forest Avenue Character Area”. Materials, methods, treatment, and type for private projects should adhere to the Design Guidelines found under Section N of the Main Street Morgantown Urban Design Document. Materials, methods, treatments, and types for public projects should adhere to Main Street Morgantown’s Design Guidelines for Public Projects found in Sections II to V. Select materials and finishes for proposed new buildings that are compatible with historic materials and finishes found in the surrounding buildings that contribute to the special character of the historic district in terms of composition, scale, module, pattern, detail, texture, finish, color, and sheen.

### Colors Palette

Warm and earth-toned colors will be encouraged predominantly. Brighter colors will be allowed but in limited accent areas.

### Architectural Style

Encourage an architectural reference for the “Forest Avenue Character Area” that draws inspiration from the many historic buildings that are part of the downtown core as described within the Main Street Morgantown Urban and Public Projects Design Guidelines. Existing building renovations, rehabilitations, and adaptive reuses will follow the Main Street Morgantown Urban and Public Projects Design Guidelines.



*Inspirational imagery for Action 6.3.1.4a taken from Bloomington, Indiana depicting a Farmer’s Market space that also serves as a parking lot during the weekdays. They have built a set of permanent overhead structures that serve as shading devices for the various users of this space throughout the week.*





Clipped from Page 90

	New Mixed Use Development		Existing Planned Development
	New Residential Development		New Street Access
	Enhanced Streetscape and setbacks		Lane or Alley Access
	Enhanced Public Space or Park		Pedestrian Bridge
	Pedestrian Street (limited auto)		Enhanced Alley or multipurpose trail
	Future Government Services Expansion		Enhanced crosswalk

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## 6.0 Downtown Strategies

### 6.6 Housing and Redevelopment

**Goal:** Redevelop vacant and underperforming properties throughout the downtown and promote a variety of mixed-use housing in order to increase density and diversify the demographics of downtown residents.

**Objectives:**

- ➔ • Increase the supply, diversity, range, and affordability of housing opportunities within the downtown.
- Increase the utilization of various tax credit programs that support the revitalization of existing buildings.
- Pursue the development of mixed-use and residential development along the Riverfront and Deckers Creek.
- Redevelop underutilized upper-floor spaces throughout the downtown to create 100-200 new housing units.

**Actions:**

- ➔ 6.6.1 Grow the downtown resident population by creating more, and a broader range of, housing opportunities. The following downtown and community-wide benefits are expected from the increase in housing:
  - Boost the captive market for community-serving retail goods and services downtown that will support new downtown residents and the residents of nearby neighborhoods.
  - Increase occupancy and mixed-uses of underutilized downtown buildings.
  - 24/7/365 living, activity, commerce, and energy will create a safer downtown.
- 6.6.2 Encourage the reuse and conversion of underutilized upper floors for new residential uses.

One of the functions of the recommended development subsidiary of Main Street Morgantown will be to provide property owners with historic rehabilitation and New Markets tax credit technical assistance that can cover a portion of rehabilitation costs. Technical assistance could be in the form of raising awareness of and participation in these and other similar financing tools; involving several property owners in aggregating their properties to make tax credit financing more efficient; identifying potential local tax credit investors; and, providing pro bono rehabilitation financing assistance.
- 6.6.3 Stimulate infill development of mixed-use buildings on vacant lots throughout the downtown.
- 6.6.4 Redevelop the areas along the Monongahela River in order to attract revitalization and infill projects in the downtown.
- 6.6.5 New housing should support and integrate a diversity of age groups and income levels.
- 6.6.6 Use development of new housing to better connect surrounding neighborhoods to the downtown.

Residents and workers in the surrounding neighborhoods pass through many of the downtown "Character Areas" when traveling to work, visiting businesses, and accessing recreational sites.

Developing housing along the Creek and the River will make these areas livelier and, in turn, will make the connections between the downtown and adjacent neighborhoods safer.

*Clipped from Pages 122 and 123*

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City of Morgantown, West Virginia

APPLICATION FOR TYPE III SITE PLAN REVIEW

OFFICE USE
CASE NO. S14-07-III
RECEIVED: 8/8/14
COMPLETE:

The Morgantown Planning Commission is responsible for approving Type III Site Plan Review Applications. There are two categories of Type III Site Plans Review Applications, 1) Developments of Significant Impact and 2) Major Developments of Significant Impact. Please check the category that best describes your proposed development:

Developments of Significant Impact (DSI):

- Residential Projects:..... 12 to 99 dwelling units
Commercial Projects: ..... 15,000 square feet of gross floor area
Office / Institution Projects:..... 15,000 square feet of gross floor area
Industrial Projects..... 0 square feet to 99,999 square feet of gross floor area
Mixed Use Projects ..... 15,000 square feet of gross floor area

Major Developments of Significant Impact (Major DSI):

- Residential Projects:..... 100 or dwelling units
Commercial Projects: ..... 100,000 or more square feet of gross floor area
Office / Institution Projects:..... 100,000 or more square feet of gross floor area
Industrial Projects..... 100,000 or more square feet of gross floor area
Mixed Use Projects ..... 100,000 or more square feet of gross floor area

CITY OF MORGANTOWN PLANNING DEPARTMENT

AUG 8 2014

RECEIVED

0107

(PLEASE TYPE OR PRINT IN BLACK INK)

I. APPLICANT
Name: CA Student Living
Phone: 312-994-1874
Mailing Address: 161 N. Clark Suite 4900
Street: Chicago IL 60601
City: State Zip
Email: dhrankowsky@ca-studentliving.com

II. AGENT / CONTACT INFORMATION
Name: Projec Management Services / Lisa Mardis
Phone: 304-212-5256
Mailing Address: 160 Fayette Street Suite 101
Street: Morgantown WV 26505
City: State Zip
Email: pms160@comcast.net

Mailings - Send all correspondence to (check one): [ ] Applicant OR [ ] Agent/Contact

III. PROPERTY
Owner: VFW Post #548
Phone: 304-292-3927
Mailing Address: 494 Spruce Street
Street: Morgantown WV 26505
City: State Zip
Email: PAID

PAID
AUG 12 2014
BY:

2605.00
\$2,520 CK 4713



City of Morgantown, West Virginia

APPLICATION FOR TYPE III SITE PLAN REVIEW

OFFICE USE	
CASE NO.	S14-07-III
RECEIVED:	
COMPLETE:	

IV. SITE			
Street Address (if assigned):	494 Spruce Street	Zoning:	B-4
Tax Map(s) #:	26	Parcel(s) #:	245 & 246
Existing Use of Structure or Land:	Fraternal organization		
Proposed Use of Structure of Land:	Mixed use / multi-family w/ required commercial		

V. STRUCTURE			
Proposed Setbacks:	Front: 16'8" ft.	Rear: 22'6" ft.	Side A: 0 ft. Side B: 2'6" ft.
Proposed Height of Structure:	110'	No. of Proposed Off-Street Parking Spaces:	170
No. of Dwelling Units (if applicable):	89	No. of Bedrooms:	331
		No. of Employees:	TBT
Square Footage of all Proposed Structures (please explain):			
Misc: Lot Coverage: 84.8% (Lot area 27,459 sf / lot coverage 23,354 sf); 97 bike storage spaces; FAR 5.82; Article 1351.01 req - 65% transparency			

VI. SITE PLAN REQUIREMENTS	
All applications for Type III Site Plan Review must be accompanied by complete and accurate site plan exhibits that meet the requirements set forth in Article 510.08 of the Zoning Ordinance and provided herein as:	
Addendum A.....	Developments of Significant Impact
Addendum B.....	Major Developments of Significant Impact

IX. ATTEST		
I hereby certify that I am the owner of record of the named property, or that this application is authorized by the owner of record and that I have been authorized by the owner to make this application as his/her authorized agent and I agree to conform to all applicable laws of this jurisdiction. I certify that the information submitted herein and attached hereto is true and accurate and understand that if found otherwise may result in the denial of this request or subsequent revocation of any and all related approvals. The undersigned has the power to authorize and does hereby authorize City of Morgantown representatives on official business to enter the subject property as necessary to process the application and enforce related approvals and conditions.		
Lisa Mardis		08/08/2014
Type/Print Name of Applicant/Agent	Signature of Applicant/Agent	Date

- Applicants will be advised of the Technical Review Committee meeting date/time.
- Site Plan Review Fee – \$75 for first \$200,000 in construction costs; \$10 for each additional \$100,000



City of Morgantown, West Virginia  
TYPE III SITE PLAN REVIEW ADDENDUM A  
DEVELOPMENTS OF SIGNIFICANT IMPACT

**Developments of Significant Impact** are those that have a citywide impact. Such impacts would typically involve the transportation network, environmental features such as parks or stream corridor, and local schools.

- (1) All applications for a Development of Significant Impact shall be accompanied by the following:
  - (a) A site plan (14 copies), drawn to scale, that includes the following for the use of the Planning Director:
    - (i) The actual dimensions, size, square footage, and shape of the lot to be built upon as shown on an actual survey by a licensed land surveyor or registered design professional licensed by the State of West Virginia and as authorized by West Virginia State law, said survey to be provided by the applicant.
    - (ii) The exact sizes and locations on the lot of existing structures, if any.
    - (iii) The location, square footage, and dimensions of the proposed structure or alteration.
    - (iv) The location of the lot with respect to adjacent rights-of-way.
    - (v) The existing and proposed uses of the structure and land.
    - (vi) The number of employees, families, housekeeping units, bedrooms, or rental units the structure is designed to accommodate.
    - (vii) The location and dimensions of off-street parking and means of ingress and egress for such space.
    - (viii) Height of structure;
    - (ix) Setbacks;
    - (x) Buffer yard and screening, if applicable;
    - (xi) Location of garbage collection area and screening;
    - (xii) Location of sign;
    - (xiii) Layout of all internal roadways;
    - (xiv) Location of stormwater management facilities;
    - (xv) Utility lines and easements; and
    - (xvi) Signature of applicant.
  - (b) Grading plans and drainage plans and calculations are not required for Planning Commission site plan review, but shall be required prior to issuance of any building permits. Such plans shall be prepared by a registered design professional licensed by the State of West Virginia, and as authorized by West Virginia State law; and shall also meet all applicable local, state and federal regulations.
  - (c) Parking plan
  - (d) Landscaping plan
  - (e) Sign plan
  - (f) Approved WV Division of Highways Access Permit, if applicable
  - (g) Any other such information concerning the lot or neighboring lots as may be required by the Planning Director to determine conformance with, and provide for the enforcement of, this ordinance; where deemed necessary, the Planning Director may require that in



City of Morgantown, West Virginia  
**TYPE III SITE PLAN REVIEW ADDENDUM A**  
**DEVELOPMENTS OF SIGNIFICANT IMPACT**

the case of accessory structures or minor additions, all dimensions shown on plans relating to the size of the lot and the location of the structure(s) thereon be based on an actual survey by a registered land surveyor or registered design professional licensed by the State of West Virginia and as authorized by West Virginia State law, said survey to be provided by the applicant.

- (3) The Planning Director may require that the lot and location of the building(s) thereon shall be staked out on the ground before construction.
- (4) For uses which, in the opinion of the City Engineer, may create excessive negative traffic impacts on dedicated City streets in the immediate vicinity that serve the use, the City may require an analysis of the proposed development's impact on current or future traffic flows, at the developer's expense, prepared by a qualified professional engineer. The Planning Commission may also table consideration of a development and refer such development to the City Engineer to ask his or her opinion as to whether a traffic study may be warranted. If the study indicates that the projected traffic impact of the use would result in a two (2) full letter grade decline in the existing Level of Service (e.g., going from a Level of Service B to a Level of Service D) of any dedicated City street directly serving the use, such finding *may* be considered sufficient grounds for denial of the project, or a requirement that sufficient improvements be made to said streets, at the developer's expense, or that the project be reduced in size and scope to the point where no such negative impact on the Level of Service results. Level of Service refers to the traffic grading system described in the latest edition of the Highway Capacity Manual, published by the Transportation Research Board.
- (5) Site plans approved by the Planning Commission authorize only the use, arrangement, and construction set forth in such approved site plans and no other use, arrangement or construction. Furthermore, the approval of a site plan shall not be construed to be approval of any violation of the provisions of this ordinance. The issuance of a building permit based upon site plans given approval by the Planning Commission shall not prevent the Planning Director from thereafter requiring the correction of errors in said site plans or from preventing operations from being carried on thereunder when in violation with this ordinance. Site plan approval does not eliminate the need to obtain an approved building permit and the applicant's responsibility to meet all other requirements established by local, state and federal regulations.
- (6) One copy of the site plan submitted for a permit to the Planning Department shall be returned to the applicant after the Planning Director has marked such copy as either approved or disapproved as to the provisions of this ordinance and attested to same by his signature on such copy. The original, similarly marked, shall be retained by the Planning Director.

I hereby certify that I have read the site plan submission requirements provided herein and understand that failure to submit said exhibits constitutes an incomplete application which will result in application review delays.

Lisa Mardis

08/08/2014

Type/Print Name of Applicant/Agent

Signature of Applicant/Agent

Date

S14-07-III



City of Morgantown, West Virginia

TYPE III SITE PLAN REVIEW ADDENDUM B  
MAJOR DEVELOPMENTS OF SIGNIFICANT IMPACT

The review process for all **Major Developments of Significant Impact** shall be identical to that for Developments of Significant Impact, except as otherwise noted in the plan submission requirements listed below.

**Major Developments of Significant Impact** are those that are of such scope and scale that they have an impact on the region in terms of the transportation network, the environment, the schools, etc. Such projects could include regional shopping centers and large scale residential developments. **All** applications for a **Major Development of Significant Impact** shall be accompanied by a site plan submitted under the seal and signature of a registered design professional licensed by the State of West Virginia and as authorized by West Virginia State law. All sheets shall be 24" x 36" size drawn to scale at a minimum 1"=50' and a maximum 1"=10' with the exception of the maps on Sheet One, unless otherwise approved by the City Engineer. Eighteen (18) copies of the site plans shall be submitted for review and shall observe the following format:

**(1) Sheet One (Title Sheet)**

The following information shall be submitted as part of Sheet One:

- (a) Full legal description with sufficient reference to section corners and boundary map of the subject project, including appropriate benchmark references;
- (b) Name of the project;
- (c) Name and address of the owner, developer, and person who prepared the plans;
- (d) Total acreage within the project and the number of residential dwelling units or the gross square footage of non-residential buildings whichever is applicable;
- (e) Existing zoning of the subject land and all adjacent lands;
- (f) Boundary lines of adjacent tracts of land, showing owners of record;
- (g) A key or vicinity map at a scale of one inch equals four hundred feet or less, showing the boundaries of the proposed project and covering the general area within which it is to be located;
- (h) A statement of the proposed uses, stating the type and size of residential and non-residential buildings, and the type of business, commercial or industry, so as to reveal the effect of the project on traffic, fire hazards, or congestion of population;
- (i) Any existing or proposed covenants and restrictions affecting property owners and/or homeowners associations; and
- (j) Statement of proposed starting and completion dates for the project, including any proposed phasing and sequencing.

**(2) Sheet Two (Existing Site Conditions)**

The following information shall be submitted as part of Sheet Two:

- (a) Location, widths, and type of construction of all existing streets, street names, alleys, or other public ways and easements, street classifications as per the approved regional transportation plan, railroad and utility rights-of-way or easements, parks, wooded areas, cemeteries, watercourses, drainage ditches, designated wetlands, low areas subject to



**City of Morgantown, West Virginia**  
**TYPE III SITE PLAN REVIEW ADDENDUM B**  
**MAJOR DEVELOPMENTS OF SIGNIFICANT IMPACT**

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flooding, permanent buildings, bridges, and other data considered pertinent by the Planning Commission or the Planning Director for the subject land, and within three hundred (300) feet of the proposed project;

- (b) Existing water mains, fire hydrants, storm sewers, sanitary sewers, culverts, bridges, and other utility structures or facilities within, adjacent to, or serving the subject land, including pipe sizes, grades, and exact locations, as can best be obtained from public or private records;
- (c) Existing contours based in U.S.G.S. datum with intervals of not more than two (2) feet. Elevations shall be based on sea level datum; and
- (d) The water elevation at the date of the survey of rivers, lakes, streams, or designated wetlands within the project or affecting it, as well as the approximate high and low water elevation of such rivers, lakes, streams, or designated wetlands. The plan shall also show the boundary line of the regulatory 100-year flood. The plan shall also show the base flood elevation of the regulatory 100-year flood at any building location along with the elevation of the lowest finished floor. All elevations shall be based on sea level datum;

**(3) Sheet Three (Proposed Site Conditions)**

The following information shall be submitted as part of Sheet Three:

- (a) Location, widths, and type of construction of all existing and proposed streets, street names, alleys, or other public ways and easements, railroad and utility rights-of-way or easements, parks, wooded areas, cemeteries, watercourses, drainage ditches, designated wetlands, low areas subject to flooding, permanent buildings, bridges, and other data considered pertinent by the Planning Commission or the Planning Director for the subject land, and within three hundred (300) feet of the proposed project;
- (b) Existing and proposed water mains, fire hydrants, storm sewers, sanitary sewers, culverts, bridges, and other utility structures or facilities within, adjacent to, or serving the subject land, including pipe sizes, grades, and exact locations, as can best be obtained from public or private records;
- (c) Building setback lines, showing dimensions;
- (d) Full description and details, including engineering calculations, for provision of storm water drainage plans and facilities, as required by the City's stormwater management ordinance;
- (e) Internal and perimeter sidewalk system/pedestrian circulation plan; and
- (f) Proposed contours with intervals of not more than two (2) feet. The plan shall also show the contour line for the floodway fringe boundary.
- (g) Show the location and detail plans for all trash dumpsters.



**City of Morgantown, West Virginia**  
**TYPE III SITE PLAN REVIEW ADDENDUM B**  
**MAJOR DEVELOPMENTS OF SIGNIFICANT IMPACT**

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**(4) Sheet Four (Erosion Control Plan)**

The following information shall be submitted as part of Sheet Four and shall be reviewed prior to issuance of a building permit:

- (a) Location, widths, and type of construction of all existing and proposed streets, street names, alleys, or other public ways and easements, railroad and utility rights-of-way or easements, parks, wooded areas, cemeteries, watercourses, drainage ditches, designated wetlands, low areas subject to flooding, permanent buildings, bridges, and other data considered pertinent by the Planning Commission or the Planning Director for the subject land, and within three hundred (300) feet of the proposed project;
- (b) Proposed contours with intervals of not more than two (2) feet.
- (c) Details of terrain and area drainage, including the identity and location of watercourses, intermittent and perennial streams, receiving waters, and springs, and the total acreage of land that will be disturbed.
- (d) The direction of drainage flow and the approximate grade of all existing or proposed streets.
- (e) Detailed plans and locations of all surface and subsurface drainage devices, walls, dams, sediment basins, storage reservoirs, and other protective devices to be constructed with, or as part of, the proposed project, together with a map showing drainage area, the complete drainage network, including outfall lines and natural drainage ways which may be affected by the proposed development, and the estimated runoff of the area served by the drains.
- (f) A description of the methods to be employed in disposing of soil and other material that is removed from the grading site, including the location of the disposal site.
- (g) Measures for soil erosion and sediment control which must meet or exceed the methods and standards adopted by the West Virginia Department of Natural Resources and/or set forth in the West Virginia Handbook For Erosion Control in Developing Areas and which must comply with the design principles, performance standards, and requirements set forth in this chapter.
- (h) A schedule of the sequence of installation of planned erosion and sediment control measures as related to the progress of the project, including the total area of soil surface that is to be disturbed during each stage, the anticipated starting and completion dates, and a schedule for the maintenance of such measures.
- (i) Include the following notes on the sheet:
  - (i) "All erosion control practices shall be in accordance with the WVDNR "West Virginia Handbook For Erosion Control In Developing Areas" dated October 1992 and the SCS "Field Office Technical Guide."
  - (ii) "The City Engineer has the right to require additional erosion control measures in the field as conditions warrant."
- (j) Copies of the letter of intent and response from the Monongalia County Soil and Water Conservation District office for compliance, when required.
- (k) Any other information reasonably required by the Planning Commission or Planning Director to properly evaluate the plan.



City of Morgantown, West Virginia  
TYPE III SITE PLAN REVIEW ADDENDUM B  
MAJOR DEVELOPMENTS OF SIGNIFICANT IMPACT

(5) Sheet Five (Landscape Plan)

A landscape plan prepared to the standards specified in this zoning ordinance.

(6) Sheet Six (Plat-like dedication sheet, if necessary)

The following information shall be submitted as part of Sheet Five if a plat-like dedication document for easements and rights-of-way is deemed necessary by the Planning Commission or its authorized designee:

- (a) Parcels of land proposed to be dedicated or reserved for public use, or reserved for common use of all property owners within the project, with the proposed conditions and maintenance requirements, if any, shall be designated as such and clearly labeled on the plans;
  - (i) Radii, internal angles, points of curvature; tangent bearings and lengths of all arcs, chord, and chord bearings; and
  - (ii) Accurate location of all survey monuments erected, corners and other points established in the field in their proper places.

(7) All sheets shall contain the following information:

- (a) The proposed name by which the project shall be legally and commonly known;
- (b) Date of survey, scale, and north point;
- (c) All lots or outlots intended for sale or lease shall be designated with boundary lines and numbered or labeled for identification purposes;
- (d) Private parks, common areas, or excluded parcels shall be designated as such and clearly labeled on the plans;
- (e) A traffic impact study, if required by the City Engineer;
- (f) Such other information as may be deemed necessary for proper review of the site plan by the Planning Director, City Engineer, or Planning Commission;
- (g) All necessary reference points tying the subject property to the appropriate section corners;
- (h) Each sheet shall be sealed and signed by the professional preparing the drawings;
- (i) All sheets shall be tied to state plane coordinates for horizontal and vertical controls;
- (j) Names and addresses of the parties within 200 feet of the property; and,
- (k) The applicant must provide self-addressed stamped envelopes in sufficient quantities to provide notification to the parties identified in the item above. Return address is not required.

I hereby certify that I have read the site plan submission requirements provided herein and understand that failure to submit said exhibits constitutes an incomplete application which will result in application review delays.		
Lisa Mardis		08/08/2014
Type/Print Name of Applicant/Agent	Signature of Applicant/Agent	Date

# 494 Spruce Street / CA Student Living



- 494 Spruce Street
- Subject Site
- Located on the corner of Willey and Spruce Streets
- Dated underutilized building



- Subject site
- Spruce Street frontage
- Entrance off of Willey for First Presbyterian no longer exists.



- Subject Site
- Willey Street side



Water Commission Building

- High Street
- 8 story building



Citizen's Bank Building

- High Street
- 7 Stories



- Monongalia Building
- High Street
- 8 Stories



- Hotel Morgan
- High Street
- 8 Stories

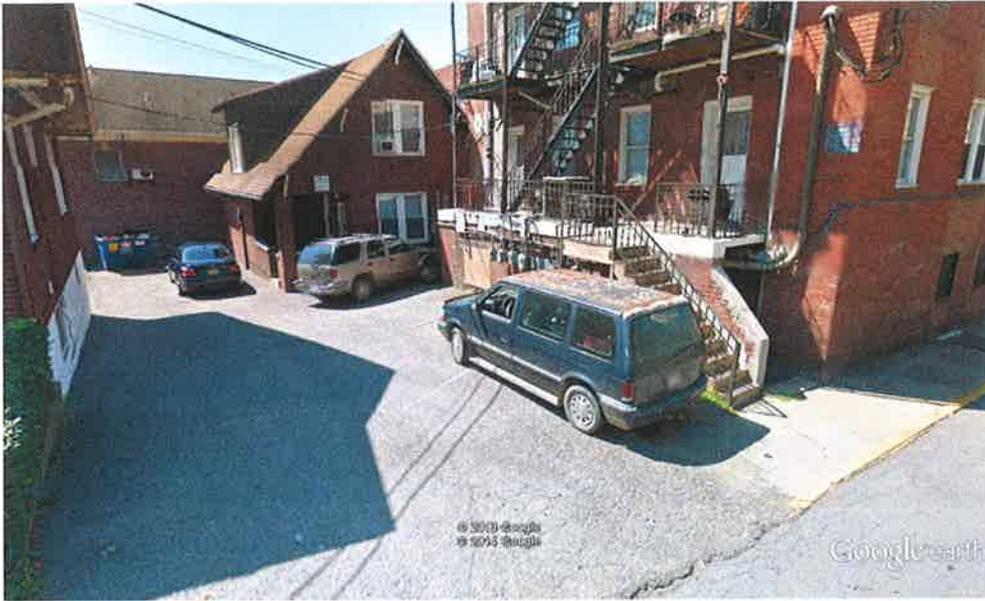


- Located at the corner of Spruce Street and Forest Avenue
- Buildings with little to no rear setback and full lot coverage



Examples of no rear setback and full lot coverage





- Located at the corner of Spruce Street and Forest Avenue
- Buildings with little to no rear setback and full lot coverage



Examples of no rear setback and full lot coverage

## Examples of Urban or Street Canyons



Magnificent Mile, Chicago IL



Canyon of Heroes, Manhattan NY



WEST VIRGINIA DEPARTMENT OF TRANSPORTATION

Division of Highways

1900 Kanawha Boulevard East • Building Five • Room 110  
Charleston, West Virginia 25305-0430 • (304) 558-3505

Earl Ray Tomblin  
Governor

Paul A. Mattox, Jr., P. E.  
Secretary of Transportation/  
Commissioner of Highways

August 8, 2014

Mr. Dan Hrankowsky  
Director of Design  
CA Student Living  
161 N Clark  
Suite 2050  
Chicago, Illinois 60601

Dear Mr. Hrankowsky:

In June 2014, the West Virginia Division of Highways (WVDOH) issued approval of a Traffic Impact Study (TIS) regarding the proposed 494 Spruce Development to be located adjacent to US 119 in Morgantown, Monongalia County. You recently notified the WVDOH that certain revisions to the scope of the project are proposed, and the WVDOH has reviewed the revised information presented to us.

The results of our review indicate that based on the information provided, the WVDOH sees no need for you to provide the WVDOH with any updates to the previously approved TIS, as the revisions are relatively negligible and no increase in trip generation is proposed. The previously approved TIS still would be in effect for WVDOH permitting purposes with no revisions necessary and the TIS would be finalized after you have addressed appropriately any comments/concerns you may receive from the City of Morgantown and/or the Greater Morgantown Metropolitan Planning Organization. As noted previously, you would need to provide the WVDOH with three printed versions and two electronic versions of the full, final, approved TIS.

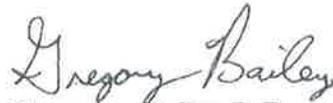
The access location along US 119 (Willey Street) appears to be the same as previously proposed and the WVDOH still is agreeable to that location, provided it has the same restriction (no left turn onto Willey Street from the site) as previously agreed. The information provided to the WVDOH also appears to indicate that the US 119 Northbound (Spruce Street) access now is proposed to be egress only from the site; the WVDOH has no objection to that but we are also still agreeable to a right-in/right-out access at Spruce Street as previously proposed. Also, please provide additional information concerning your proposed Option 1. Our understanding previously was that the bottom level of parking within the site was to be accessible only from Spruce Street. If the Spruce Street access is to be egress only, will the lower level of parking remain and if so, will it now be accessible from Willey Street?

**Mr. Dan Hrankowsky**  
**August 8, 2014**  
**Page Two**

The recommendations and conclusions of the TIS, once finalized, are to be incorporated into the Plans. When desired, you may submit to the WVDOH four sets of the Project Plans and any associated drainage calculations for review by WVDOH as part of our Plan approval/permitting process.

Thank you for your assistance with this matter. Should you require additional information, please contact Mr. David E. Cramer, P. E., of our Commissioner's Office of Economic Development, at (304) 558-9211.

Very truly yours,

  
Gregory L. Bailey, P. E.  
State Highway Engineer

GLB:Cb

cc: **Mr. Damien Davis, City of Morgantown**  
**Mr. Chris Fletcher, City of Morgantown**

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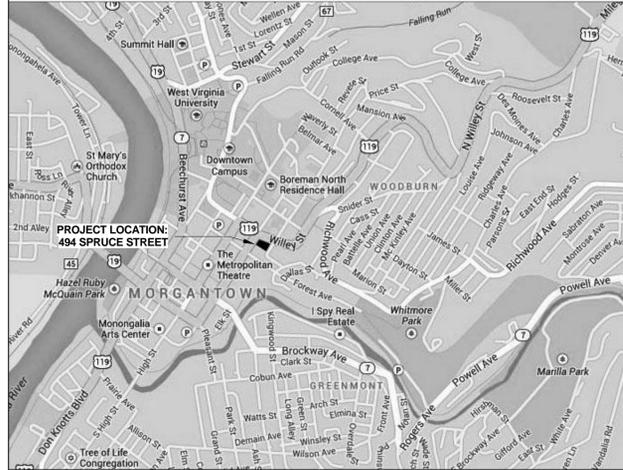
# SYMBOLS

	ROOM NAME ROOM NUMBER
	ROOM NAME ROOM NUMBER
	ROOM WALL FINISH, UNO ROOM BASE FINISH, UNO ROOM FLOOR FINISH, UNO
	PROJECT KEYNOTE (BASED ON CSI FORMAT)
	PARTITION TYPE TAG
	DOOR NUMBER HARDWARE SET DOOR GROUP
	WINDOW TAG
	LOUVER TAG
	WORK POINT / DATUM

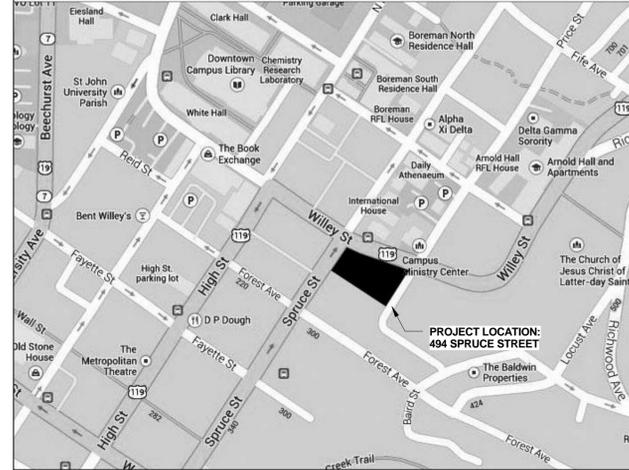
# MATERIALS

	EXISTING		METAL
	COMPACTED EARTH/FILL		GYPSUM WALL BOARD
	GRAVEL		RIGID INSULATION
	CONCRETE		BATT INSULATION
	CONCRETE MASONRY UNIT		CONTINUOUS BLOCKING
	STONE		DISCONTINUOUS BLOCKING
	BRICK		PLYWOOD
	GROUT		FINISH WOOD

# VICINITY MAP



# LOCATION MAP



# DRAWING INDEX

Sheet No.	Sheet Name
Z0.01	Cover Sheet
CIVIL	
SHEET 1	TITLE SHEET
SHEET 2	EXISTING SITE CONDITIONS
SHEET 3A	DEMOLITION PLAN
SHEET 3B	PROPOSED SITE CONDITIONS
SHEET 4	EROSION CONTROL PLAN
SHEET 6	DETAILS
SHEET 7	DETAILS
SHEET 8	DETAILS
LANDSCAPE	
L1.0	Landscape Plan and Details
ARCHITECTURAL	
Z0.02	Ground Floor Plans - Levels P0-01
Z0.03	Residential Floor Plans - Levels 02-09
Z0.04	Building Elevations
Z0.12	Perspectives & Transparency Study
Z0.13	Lighting Plans

Drawing Issue	Date
PC/BZA SUBMISSION	2014-04-04
PC/BZA SUBMISSION 2	2014-08-08

494 Spruce Street  
Morgantown, WV

### Client:



Campus Acquisitions Holdings, LLC  
161 Clark Suite 2050  
Chicago, IL 60601  
312-994-1874

### Landscape Architect:

RoofMeadow  
7135 Germantown Avenue  
2nd Flr  
Philadelphia, PA 19119

### MEP/FP/IT Engineer:

EnviroNetics Design, Inc.  
One Penn Center  
1617 JFK Blvd, Suite 1600  
Philadelphia, PA 19103

### Structural Engineer:

O'Donnell & Naccarato  
111 S. Independence Mall East  
Suite 950  
Philadelphia, PA 19106

### Civil Engineer

Alpha Associates, Inc.  
209 Prarie Avenue  
Morgantown, WV 26501-5934

DAVID McHENRY, AIA LIC. # NO-4615  
REGISTERED ARCHITECT - STATE OF WEST VIRGINIA

Architect: SCOTT A. ERDY, AIA  
DAVID S. McHENRY, AIA

## emArchitecture

Erdy McHenry Architecture, LLC  
915 North Orianna Street  
Philadelphia, Pennsylvania 19123  
ph: 215.925.7000 fax: 215.925.1990  
web: http://www.em-arc.com

## Cover Sheet

SCALE: 1/4" = 1'-0"	DATE: 06/11/2014
DRAWN BY: AR / LS	PROJECT NO: 1149.00

DRAWING NUMBER:

# PLANNING AND ZONING CODE ANALYSIS SUMMARY

Reference	Permitted/Required	Actual/Proposed	Comments
<b>ZONING DISTRICT</b>	Official Zoning Map 1331.01 (A) B-4 General Business		
<b>PERMITTED LAND USES</b>	TABLE 1331.05.01 1331.06 (26) (a)	Dwelling, Mixed Use The commercial or office space shall not be less than 20 percent and not more than 60 percent of the ground floor area.	<b>Mixed Use - Residential and Retail</b> <b>Ground Floor Area Commercial or Office Space</b> 15,601 SF 7,310 SF 46.9%
<b>LOT PROVISIONS</b>	1349.03 (D)	Maximum lot coverage = 90% (Lot Area = 27,459 SF)	<b>Lot Coverage</b> 84.8% (23,354 SF)
<b>SETBACKS AND ENCROACHMENTS</b>	1349.04 (A) (1) 1349.04 (A) (2) 1349.04 (A) (4) 1349.04 (A) (5)	No minimum front or street side building setback is required. The maximum front and street side building setback may not exceed the average front yard depth of the nearest two (2) lots on either side of the subject lot or 10 feet, whichever is less. No interior side setbacks are required for the first floor. The minimum rear setback shall be ten percent (10%) of the lot depth or ten (10) feet, whichever is greater.	<b>Min Front Setback</b> 00'-0" <b>Max Front Setback</b> 10'-0" <b>Min Side Setback</b> 00'-0" <b>Spruce Street Front</b> 22'-6" (East Side)
<b>BUILDING HEIGHT</b>	1349.05 (B)	The maximum height of a principal structure shall not exceed 120 feet.	<b>Building Height</b> 110'-0" Average Grade = 935 FT, Building Height = 1045 FT
<b>FLOOR AREA RATIO (FAR)</b>	1349.06	The maximum FAR for all development in this district is 7.0. Area designed, constructed, and utilized to provide parking structure facilities shall be exempt from the maximum FAR, provided such area does not exceed 115% of the minimum parking requirement. (Lot Area = 27,459 SF; Allowable Floor Area = 192,213 SF)	<b>Total Floor Area</b> 159,633 SF (5.81 FAR)
<b>LOT AREA PER DWELLING UNIT</b>	1349.07	The minimum lot area per dwelling unit in this district is 300 square feet. (Lot Area = 27,459 SF; Allowable Number of Units = 92)	<b>Number of Units</b> 89
<b>SAFETY AND VISION</b>	1363.02	Clear vision triangle of the area of the lot twenty-five (25) feet along the property line from the street right-of-way at intersections.	
<b>PARKING AND LOADING STANDARDS</b>	1349.08 (A) (1) Table 1365.04.01 1349.08 (A) (1)	Residential: Parking shall not be required for the first twenty-two (22) occupants. With the exception of the first twenty-two (22) occupants, the minimum number of parking spaces for permitted residential uses shall be one-half a space (0.5) per occupant, as determined by the West Virginia State Building Code and adopted and implemented by the City. Required Residential Parking = 155 Spaces Bicycle Storage: One (1) indoor, secured, sheltered bicycle storage space per dwelling unit (89 spaces required).	<b>Parking Provided</b> 158 Based on 331 Bed Count <b>Bicycle Spaces Provided</b> 97
<b>PERFORMANCE STANDARDS</b>	1351.101 (J) (2) (b) 1351.101 (K) (1)	All nonresidential floor space provided on the ground floor of a mixed-use building must contain at least 20 percent of the lot area on lots with 50 feet of street frontage or more. (Lot Area = 27,459 SF; Minimum Area = 5,492 SF) Transparency: Min (60%) of the street-facing building façade between 3'-0" and 8'-0".	<b>Lot Area Nonresidential Space</b> 27,459 SF 7,310 SF 27% <b>Transparency</b> 64%
<b>LOADING REQUIREMENTS</b>	1365.10 (b) Table 1365.10.01	For local pick-up and delivery trucks: 12'-0" w x 30'-0" l with 45'-0" maneuvering apron 12'-0" h clearance. Type II: 1 for each 20,000 above 100,000 5 Loading Spaces Required	<b>Loading Spaces Provided</b> 4 STANDARD LOADING + 1 TRUCK LOADING = 5 LOADING SPACES

# UNIT MIX & GSF SUMMARY

UNIT TYPES	4 x 4	4 x 2	4 x 4 Loft	3 x 2	2 x 2	Units / Floor	Beds / Floor
Level 09	3	1	0	0	5	9	26
Level 08	3	1	3	0	5	12	38
Level 07	8	3	0	1	0	12	47
Level 06	8	3	0	1	0	12	47
Level 05	8	3	0	1	0	12	47
Level 04	8	3	0	1	0	12	47
Level 03	8	3	0	1	0	12	47
Level 02	6	2	0	0	0	8	32
	52	19	3	5	10	89	331
%	59%	21%	3%	6%	11%		
AREA	1,137 SF	1,244 SF	1,552 SF	979 SF	813 SF		

PARKING	Size	Count
Level 01	UNIVERSAL SPACE	1
	STANDARD 9'-0"x18'-0"	21
	COMPACT 7'-0"x16'-0"	13
Level P2	STANDARD 9'-0"x18'-0"	37
	COMPACT 7'-0"x16'-0"	7
Level P1	STANDARD 9'-0"x18'-0"	17
	COMPACT 7'-0"x16'-0"	11
Level P0	STANDARD 9'-0"x18'-0"	46
	COMPACT 7'-0"x16'-0"	5
<b>TOTAL PARKING SPACE</b>		158
		161
		8 ACCESSIBLE SPACES AND 1 UNIVERSAL / VAN SPACE INCLUDED WITH MOTORCYCLE SPACES

STANDARD LOADING:	4
TRUCK LOADING ZONE:	1
<b>TOTAL PARKING + LOADING:</b>	<b>166</b>

RESIDENTIAL	NONRESI.	PARKING	TOTALS
Level 09	15,850 SF		15,850 SF
Level 08	15,777 SF		15,777 SF
Level 07	18,262 SF		18,262 SF
Level 06	18,262 SF		18,262 SF
Level 05	18,262 SF		18,262 SF
Level 04	18,262 SF		18,262 SF
Level 03	18,262 SF		18,262 SF
Level 02	16,929 SF		21,093 SF
Level 01	2,465 SF	2,185 SF	14,814 SF
Level P2	2,176 SF	1,082 SF	16,131 SF
Level P1	2,647 SF	4,042 SF	14,336 SF
Level P0	1,003 SF		19,009 SF
	148,158 SF	7,310 SF	64,290 SF
<b>TOTAL GROSS AREA</b>			<b>223,923 SF</b>
<b>TOTAL GROSS AREA (excluding parking, FAR = 5.81)</b>			<b>159,633 SF</b>

BICYCLE STORAGE	Type	Count	Comments
	Bike Storage Double Sided	76	
	Bike Storage Wall Mounted	21	
<b>TOTAL BIKE STORAGE SPACE</b>		<b>97</b>	

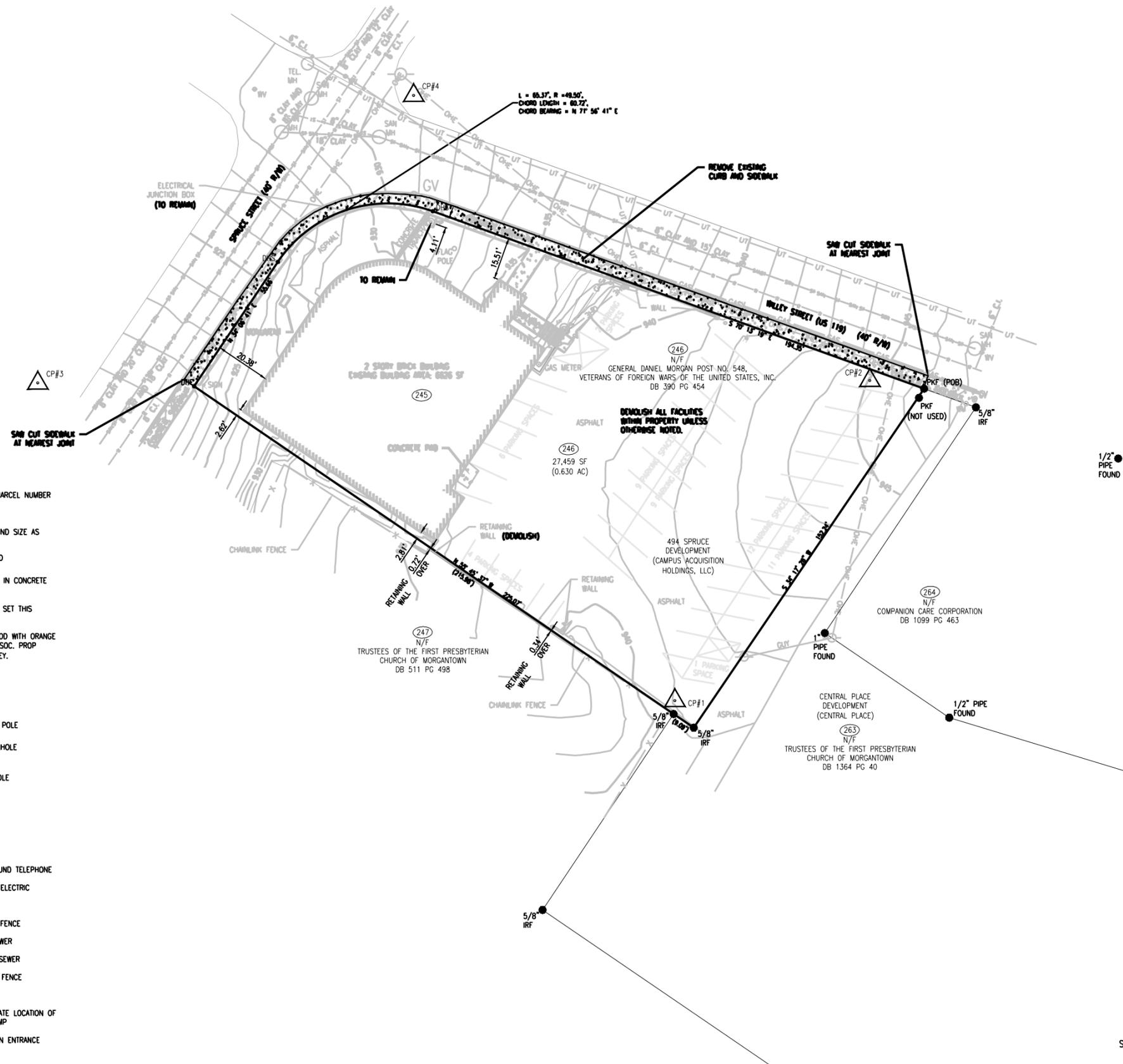
# 494 Spruce Street





CONTROL POINTS				
BENCHMARK	NORTHING	EASTING	ELEVATION	DESCRIPTION
CP#1	412310.74	1841054.80	941.06	PK NAIL
CP#2	412429.44	1841127.35	945.05	PK NAIL
CP#3	412428.47	1840817.51	921.64	PK NAIL
CP#4	412535.10	1840957.48	930.81	PK NAIL

HORIZONTAL DATUM: NAD 83 WEST VIRGINIA NORTH STATE PLANE  
 VERTICAL DATUM: NGVD 29  
 NOTE: THE LOCATION, TYPE, AND SIZES OF POTABLE WATERLINES, STORM SEWERS, AND SANITARY SEWERS ARE APPROXIMATE, BASED ON SYSTEM MAPS PROVIDED BY THE MORGANTOWN UTILITY BOARD.



**DRAWING KEY**

- XXX INDICATES TAX MAP 26 PARCEL NUMBER
- N/F NOW OR FORMERLY
- IRF INDICATES IRON ROD FOUND SIZE AS INDICATED
- PKF INDICATES PK NAIL FOUND
- DHF INDICATES 1/2" DRILL HOLE IN CONCRETE FOUND.
- DHS INDICATES 1/2" DRILL HOLE SET THIS SURVEY
- IRS INDICATES 1/2"x30" IRON ROD WITH ORANGE CAP STAMPED "ALPHA ASSOC. PROP CORNER" SET THIS SURVEY.
- EXISTING UTILITY POLE
- LP EXISTING LIGHT POLE
- TSP EXISTING TRAFFIC SIGNAL POLE
- TEL EXISTING TELEPHONE MANHOLE
- SAN MH EXISTING SANITARY MANHOLE
- GV EXISTING GAS VALVE
- WV EXISTING WATER VALVE
- WM EXISTING WATER METER
- EXISTING UNDERGROUND TELEPHONE
- E EXISTING OVERHEAD ELECTRIC
- GAS EXISTING GAS LINE
- EXISTING CHAINLINK FENCE
- SS EXISTING STORM SEWER
- SSW EXISTING SANITARY SEWER
- FFS FILTER FABRIC SILT FENCE
- IP INLET PROTECTION
- DENOTES APPROXIMATE LOCATION OF ADA COMPLIANT RAMP
- ROCK CONSTRUCTION ENTRANCE

494 SPRUCE STREET  
 FOR  
 CAMPUS ACQUISITIONS HOLDINGS, LLC  
 MORGANTOWN, WEST VIRGINIA

REVISIONS	
ITEM	DATE

PROJ. NO.: 1310125.01  
 DATE: 08/06/2014  
 SHEET NO.:



ALPHA ASSOCIATES, INC.  
 209 PRAIRIE AVENUE  
 MORGANTOWN, WV 26501  
 PHONE/FAX: 304-294-8216  
 TOLL FREE: 800-440-8216  
 www.thinkALPHAFirst.com

SCALE: 0 20'

DEMOLITION PLAN

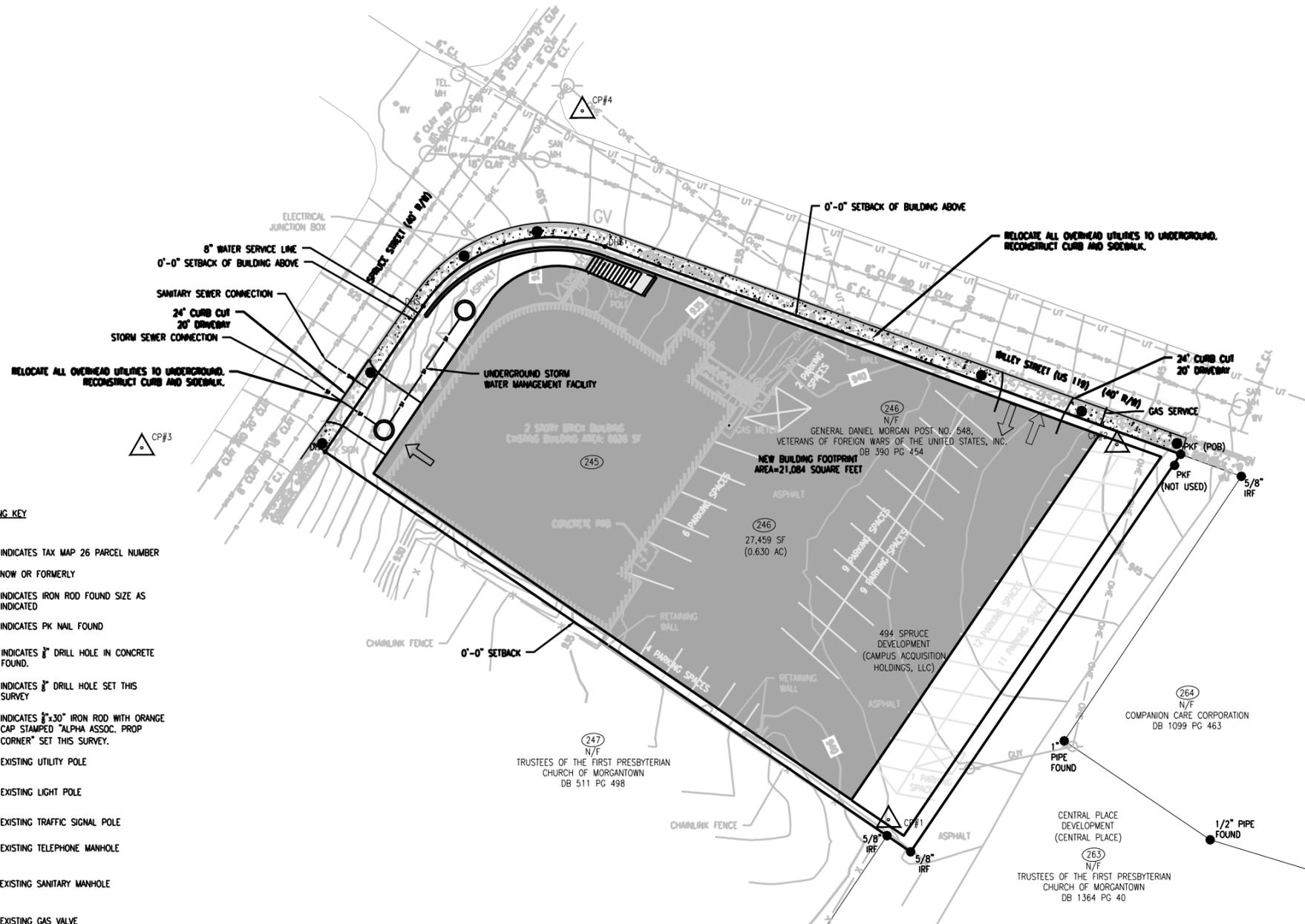
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ISSUED FOR: SCHEMATIC DESIGN

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CP#1	412310.74	1841054.80	941.06	PK NAIL
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CP#4	412535.10	1840957.48	930.81	PK NAIL

HORIZONTAL DATUM: NAD 83 WEST VIRGINIA NORTH STATE PLANE  
 VERTICAL DATUM: NAVD 29  
 NOTE: THE LOCATION, TYPE, AND SIZES OF POTABLE WATERLINES, STORM SEWERS, AND SANITARY SEWERS ARE APPROXIMATE, BASED ON SYSTEM MAPS PROVIDED BY THE MORGANTOWN UTILITY BOARD.



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494 SPRUCE STREET  
 FOR  
 CAMPUS ACQUISITIONS HOLDINGS, LLC  
 MORGANTOWN, WEST VIRGINIA

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**Alpha**  
 ARCHITECTS ENGINEERS

ALPHA ASSOCIATES, INC.  
 209 PRAIRIE AVENUE  
 MORGANTOWN, WV 26501  
 PHONE/FAX: 304-294-8214  
 TOLL FREE: 800-440-8214  
 www.thinkALPHAfirst.com

REVISIONS	
ITEM	DATE

PROJ. NO.: 1310125.01  
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PROPOSED SITE CONDITIONS

3B

ISSUED FOR: SCHEMATIC DESIGN

CONTROL POINTS				
BENCHMARK	NORTHING	EASTING	ELEVATION	DESCRIPTION
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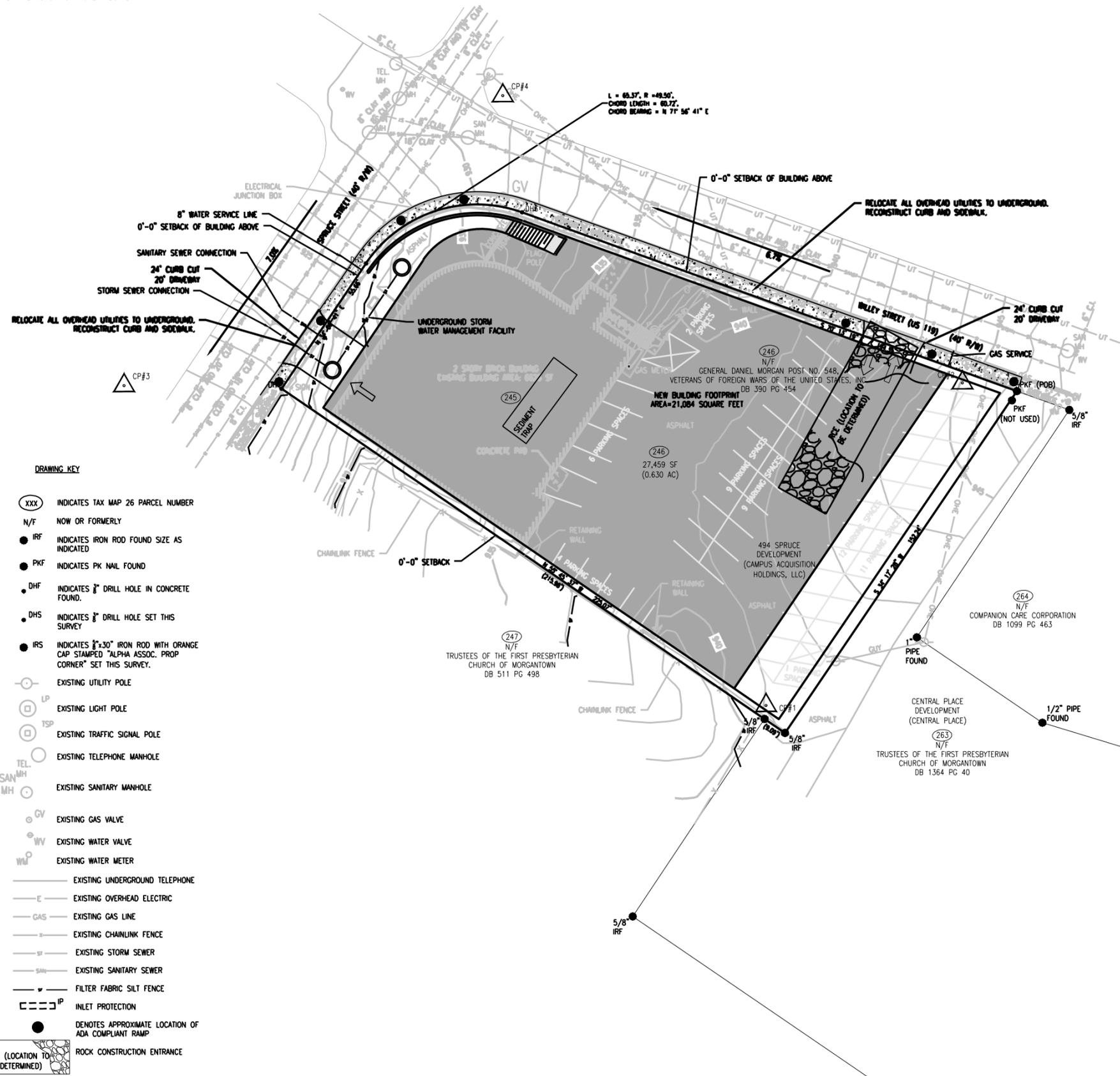
HORIZONTAL DATUM: NAD 83 WEST VIRGINIA NORTH STATE PLANE  
 VERTICAL DATUM: NOVD 29  
 NOTE: THE LOCATION, TYPE, AND SIZES OF POTABLE WATERLINES, STORM SEWERS, AND SANITARY SEWERS ARE APPROXIMATE, BASED ON SYSTEM MAPS PROVIDED BY THE MORGANTOWN UTILITY BOARD.



- EROSION CONTROL NOTES:**
- DISPOSAL SITE FOR EXCESS EXCAVATED MATERIALS TO BE DETERMINED.
  - ALL EROSION CONTROL PRACTICES SHALL BE IN ACCORDANCE WITH THE WVDNR "WEST VIRGINIA HANDBOOK FOR EROSION CONTROL IN DEVELOPING AREAS" DATED OCTOBER 1992 AND THE SCS "FIELD OFFICE TECHNICAL GUIDE."
  - THE CITY ENGINEER HAS THE RIGHT TO REQUIRE THE ADDITIONAL EROSION CONTROL MEASURES IN THE FIELD AS CONDITIONS WARRANT.

**SEQUENCE OF INSTALLATION OF PLANNED EROSION AND SEDIMENT CONTROL MEASURES**

- GENERAL**
- INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THE WEST VIRGINIA BEST MANAGEMENT PRACTICES MANUAL AVAILABLE ON THE WVDNR WEBSITE. ALL CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR AT LAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF ANY STORM EVENT OF 0.25 INCHES OR GREATER, SUBJECT TO THE PROVISIONS IN THE NOTES ABOVE.
  - UPON STABILIZATION OF ENTIRE SITE, REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
- DEMOLITION PHASE - APPROXIMATELY MAY 1 THROUGH MAY 15, 2014**
- INSTALL FILTER FABRIC SILT FENCE IN ACCORDANCE WITH DETAILS PRIOR TO COMMENCING DEMOLITION. OPERATE AND MAINTAIN FILTER FABRIC SILT FENCE IN ACCORDANCE WITH DETAILS.
  - INSTALL ROCK CONSTRUCTION ENTRANCE IN ACCORDANCE WITH DETAILS UPON REMOVAL OF ASPHALT AND EXPOSURE OF SOIL THAT COULD BE TRACKED OFFSITE DURING DEMOLITION AND EXCAVATION.
  - UPON DEMOLITION OF EXISTING BUILDING, CONSTRUCT A SEDIMENT TRAP IN ACCORDANCE WITH DETAILS IN THE EXISTING BUILDING FOOTPRINT TO COLLECT RUNOFF FROM THE SITE.
- CONSTRUCTION PHASE - APPROXIMATELY MAY 16, 2014 THROUGH JULY 2015**
- OPERATE AND MAINTAIN FILTER FABRIC SILT FENCE IN ACCORDANCE WITH DETAILS.
  - OPERATE AND MAINTAIN ROCK CONSTRUCTION ENTRANCE. RELOCATE AS NECESSARY. REMOVE UPON COMPLETION OF EXCAVATION.
  - OPERATE AND MAINTAIN SEDIMENT TRAP IN ACCORDANCE WITH DETAILS UNTIL SUCH TIME AS GROUND FLOOR CONSTRUCTION HAS STABILIZED MOST OF THE SITE.
  - INSTALL, OPERATE, AND MAINTAIN INLET PROTECTION IN ACCORDANCE WITH DETAILS AT ALL NEW INLETS AND TRENCH DRAINS.



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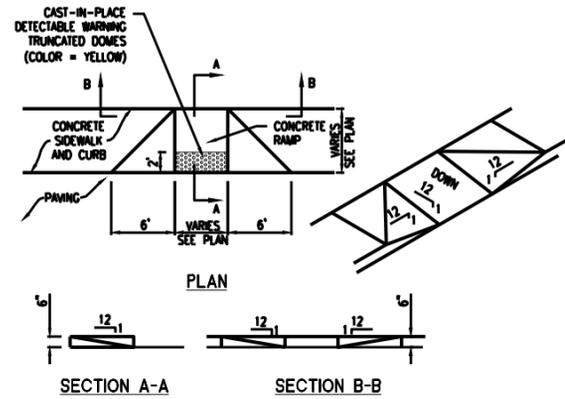


EROSION CONTROL PLAN

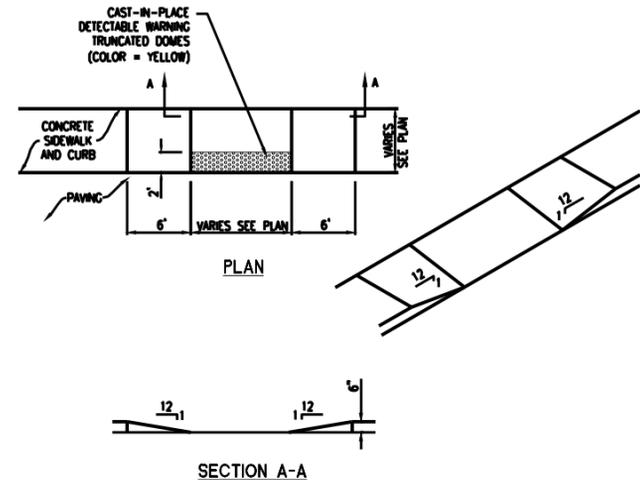
SCHEMATIC DESIGN

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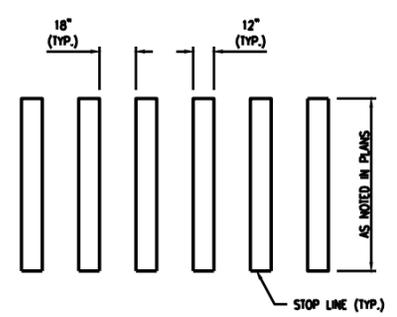
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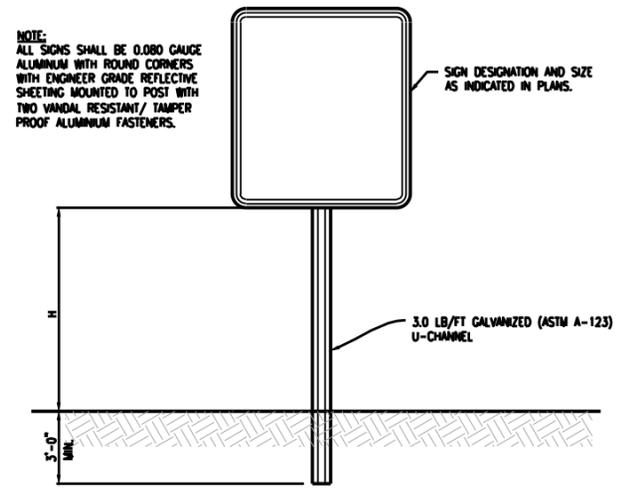
SIDEWALK RAMP DETAILS  
NO SCALE



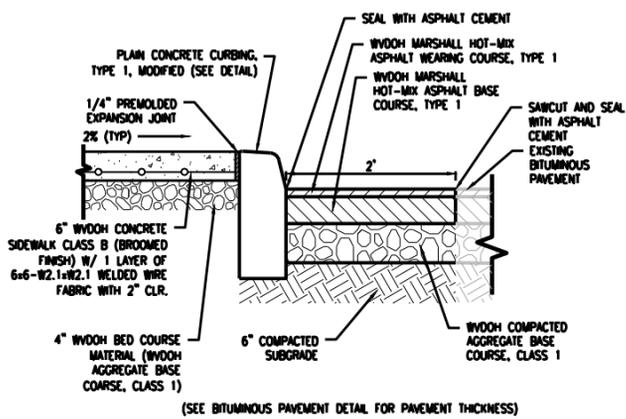
CROSS WALK DETAIL  
NO SCALE



NOTE:  
1. ALL STOP LINE USED FOR CROSSWALK SHALL BE WOOD TYPE V (SECTION 715.40.2).

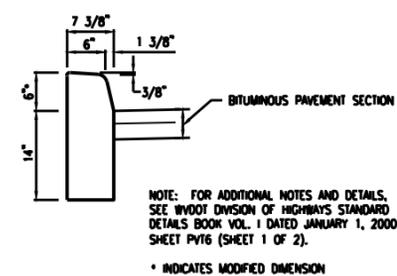


SIGN DETAIL  
NO SCALE



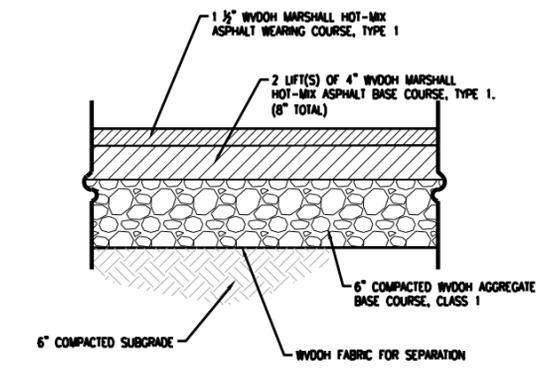
- NOTES:
- FOR SIDEWALK, PROVIDE 3/8" WIDE CONTROL JOINTS TO A DEPTH OF 1/4TH THE SIDEWALK THICKNESS AT INTERVALS EQUAL TO THE SIDEWALK WIDTH, AT ALL CHANGES OF DIRECTION, AND AT THE BEGINNING AND ENDING OF ALL TAPERED CURBS.
  - FOR CURBS, PROVIDE 3/8" WIDE CONTROL JOINTS TO A DEPTH OF 2" MINIMUM IN UNIFORM INTERVALS NOT TO EXCEED 20 FEET, AT ALL CHANGES OF DIRECTION, AND AT THE BEGINNING AND ENDING OF ALL TAPERED CURBS.
  - PROVIDE 1/4" PREMOLDED EXPANSION JOINT MATERIAL AT INTERVALS OF 30' MAXIMUM, AT THE END OF THE WORK DAY, AND AT ALL STRUCTURES AND RIGID SURFACES.
  - AROUND UTILITY POLES, METERS, FIRE HYDRANTS, AND OTHER LIKE FEATURES, CONSTRUCT AN INDEPENDENT RECTANGULAR-SHAPED SLAB A MINIMUM OF 1' IN ALL DIRECTIONS. PROVIDE 1/4" PREMOLDED EXPANSION JOINT MATERIAL AT ALL INTERFACES WITH ADJOINING CONCRETE.
  - ALL JOINTS SHALL BE NEATLY FILLED WITH WHITE ELASTOMERIC SEALING COMPOUND TO WITHIN 1/8" OF THE SURFACE.
  - ANY EXISTING FACILITIES INCLUDING, BUT NOT LIMITED TO, PARKING METERS AND FLAG POLE HOLDERS IN THE SIDEWALK REQUIRING REINSTALLATION SHALL BE REINSTALLED BY THE CONTRACTOR AT NO ADDITIONAL COST.
  - CURB AND SIDEWALK MAY BE POURED INTEGRALLY UNLESS OTHERWISE NOTED OR REQUIRED.

SIDEWALK AND CONCRETE CURBING 6" REVEAL (TYP.)  
NO SCALE

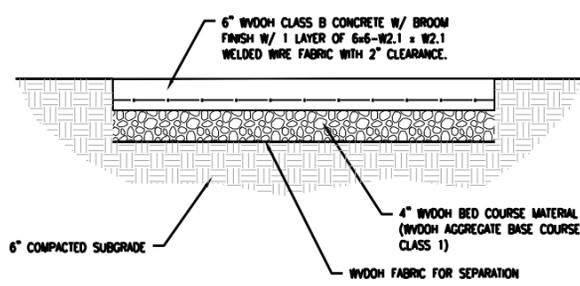


NOTE: FOR ADDITIONAL NOTES AND DETAILS, SEE WOOD DIVISION OF HIGHWAYS STANDARD DETAILS BOOK VOL. 1 DATED JANUARY 1, 2000, SHEET PY16 (SHEET 1 OF 2).  
\* INDICATES MODIFIED DIMENSION

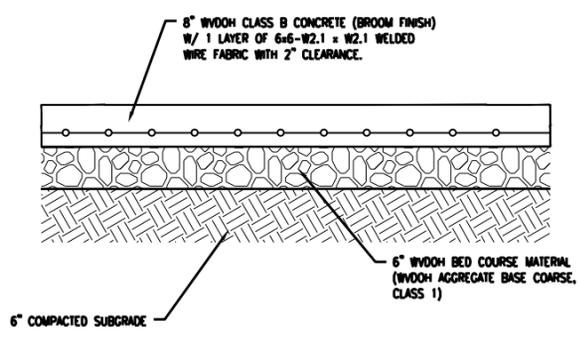
PLAIN CONCRETE CURBING, TYPE I, MODIFIED  
NO SCALE



(LOCATION) BITUMINOUS PAVEMENT DETAIL  
NOT TO SCALE



CONCRETE SIDEWALK DETAIL  
NO SCALE



CONCRETE PAD DETAIL  
NO SCALE

- SIGNING AND PAVEMENT MARKING NOTES**
- ALL 4" AND 6" SOLID WHITE OR SOLID YELLOW EDGE LINES, LANE LINES, CENTERLINES AND BARRIER LINES SHALL BE TYPE II PAVEMENT MARKING MATERIAL.
  - ALL STOP LINES SHALL BE 12" OR 24" WIDE, TYPE V, UNLESS OTHERWISE NOTED.
  - EDGE LINES SHALL BE PLACED BASED ON PLANNED LANE WIDTHS (12" TYP.) AND NOT THE EDGE OF PAVEMENT.
  - ALL MARKINGS SHALL BE CONTINUOUS AND CONSISTENT WITH EXISTING MARKINGS WHERE THEY JOIN.
  - NO MARKINGS SHALL BE PLACED ON EXPANSION JOINTS OR STRUCTURES OR ON LONGITUDINAL CONSTRUCTION JOINTS UNLESS SO DIRECTED BY THE ENGINEER.
  - CONTRACTOR SHALL NOTIFY THE ENGINEER AND WOOD PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS TO VERIFY LOCATION.



ALPHA ASSOCIATES, INC.  
209 PRAIRIE AVENUE  
MORGANTOWN, WV 26501  
PHONE/FAX: 304-294-8216  
TOLL FREE: 800-440-8216  
www.thinkALPHAfirst.com

REVISIONS

ITEM	DATE

PROJ. NO.: 1310125.01  
DATE: 08/06/2014  
SHEET NO.:

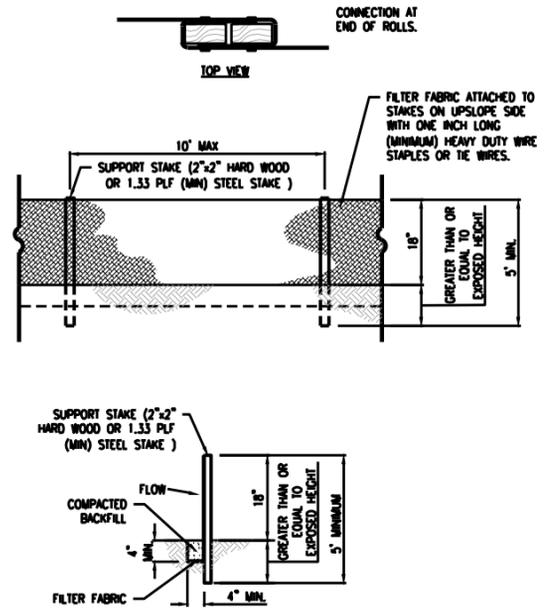
DETAILS

494 SPRUCE STREET  
FOR  
CAMPUS ACQUISITIONS HOLDINGS, LLC  
MORGANTOWN, WEST VIRGINIA

ISSUED FOR: SCHEMATIC DESIGN



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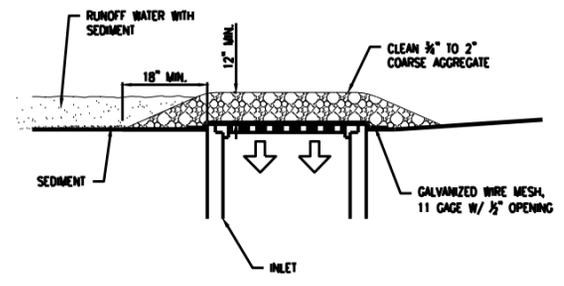


18' FILTER FABRIC SILT FENCE  
NO SCALE

- NOTES:**
1. FILTER FABRIC SILT FENCE SHALL BE CONSTRUCTED PARALLEL TO GROUND CONTOUR.
  2. BOTH ENDS OF THE FILTER FABRIC SILT FENCE SHALL EXTEND A MINIMUM OF 8 FEET UP SLOPE AT 45 DEGREES TO THE MAIN ALIGNMENT.
- MAINTENANCE:**
1. SILT FENCE SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL OF 0.5 INCH OR GREATER AND AT LEAST DAILY DURING PROLONGED RAINFALL OR ONCE A WEEK. ANY REQUIRED REPAIRS OR MAINTENANCE SHALL BE MADE IMMEDIATELY.
  2. CLOSE ATTENTION SHALL BE PAID TO THE REPAIR OF THE DAMAGED SILT FENCE RESULTING FROM END RUNS AND UNDERCUTTING. IF THE FENCE IS NOT INSTALLED ON THE CONTOUR (PERPENDICULAR TO THE FLOW OF THE WATER) BOTH OF THESE CONDITIONS CAN OCCUR.
  3. SHOULD THE FABRIC ON A SILT FENCE DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER STILL IS NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.
  4. SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH STORM EVENT. THEY MUST BE REMOVED WHEN DEPOSITS REACH APPROXIMATELY ONE HALF THE HEIGHT OF THE BARRIER.
  5. IF ANY SECTION OF SILT FENCE IS KNOCKED DOWN DURING A RAIN EVENT (BECAUSE IT WAS INSTALLED IN AN AREA OF CONCENTRATED FLOW) THEN OTHER MEASURES SUCH AS SEDIMENT TRAP AND DIVERSION OR SUPER SILT FENCE MUST BE INSTALLED.

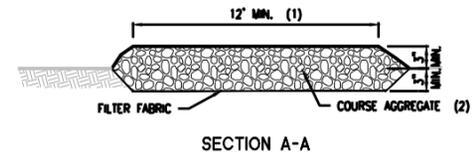
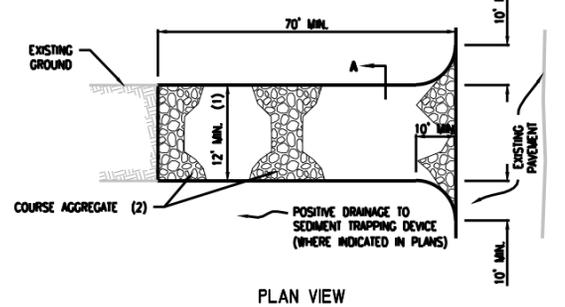
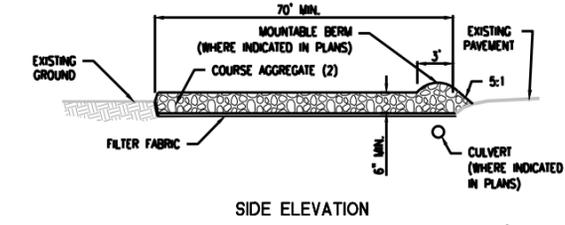
**EROSION AND SEDIMENT CONTROL NOTES**

1. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THE WEST VIRGINIA BEST MANAGEMENT PRACTICES MANUAL, AVAILABLE ON THE WVOEP WEBSITE <http://www2.wvdep.org/DEM/stormwater/BMP/index.html>. ALL CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF ANY STORM EVENT OF 0.25 INCHES OR GREATER.
2. PRIOR TO CLEARING AND GRUBBING AND BEGINNING EARTH WORK, INSTALL, OPERATE, AND MAINTAIN FILTER FABRIC SILT FENCE, "STABILIZED CONSTRUCTION ENTRANCE, AND "INLET PROTECTION" ON ALL EXISTING INLETS. AS NEW INLETS ARE CONSTRUCTED AND BECOME OPERATIONAL, INSTALL, OPERATE, AND MAINTAIN "INLET PROTECTION" AROUND THOSE INLETS.
3. UPON STABILIZATION OF ENTIRE SITE, REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES AND SEED AND MULCH THOSE AREAS DISTURBED BY THEIR REMOVAL IN ACCORDANCE WITH THE SPECIFICATIONS.
4. INSTALL, OPERATE, AND MAINTAIN FILTER FABRIC SILT FENCE AS PER DETAIL.
5. "STABILIZED CONSTRUCTION ENTRANCE" SHALL BE COMPLETE IN PLACE AS PER DETAIL AND SHALL INCLUDE ALL OPERATIONS AND MAINTENANCE.
6. "INLET PROTECTION" SHALL BE COMPLETE IN PLACE AS PER DETAIL AND SHALL INCLUDE ALL OPERATIONS AND MAINTENANCE.
7. ALL TOP SOIL IN THE DISTURBED AREAS SHALL BE STRIPPED AND STORED FOR USE IN AREAS RECEIVING PERMANENT VEGETATION.
8. AS REQUIRED UNDER PERMIT WVO115924, STABILIZATION MEASURES, INCLUDING, BUT NOT LIMITED TO, PERMANENT SEEDING AND MULCHING SHALL BE INITIATED AS SOON AS PRACTICAL IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN SEVEN (7) DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
9. "PERMANENT SEEDING AND MULCHING" SHALL BE COMPLETE AND IN PLACE IN ACCORDANCE WITH WVOEP SPECIFICATION SECTION 652 AND SHALL INCLUDE THE FOLLOWING:
  - SEED MIX, TYPE C-2 APPLIED AT A RATE OF 97 POUNDS PER ACRE.
  - STRAW MULCH APPLIED AT A RATE OF 2 TONS PER ACRE.
  - FERTILIZER APPLIED AT A RATE OF 1000 POUNDS PER ACRE OF 10-20-10 FERTILIZER OR EQUIVALENT.
  - SLOW RELEASE UREA FORMALDEHYDE FERTILIZER AT A RATE OF 300 POUNDS PER ACRE.
  - AGRICULTURAL LIMESTONE APPLIED AT A RATE OF 1.5 TON PER ACRE.



INLET PROTECTION  
NO SCALE

- NOTES:**
1. OTHER EFFECTIVE METHODS OF PROVIDING INLET PROTECTION MAY BE USED UPON THE ENGINEERS REVIEW.
- MAINTENANCE:**
1. THE STRUCTURE SHALL BE INSPECTED AFTER EACH 0.5" OF RAIN AND AT LEAST ONCE A WEEK AND REPAIRS MADE AS NEEDED. CONSTRUCTION TRAFFIC HAS A TENDENCY TO DESTROY THESE PRACTICES SO FREQUENT INSPECTIONS ARE NECESSARY.
  2. SEDIMENT SHALL BE REMOVED AND THE TRAP RESTORED TO ITS ORIGINAL DIMENSIONS WHEN THE SEDIMENT HAS ACCUMULATED TO ONE HALF THE DESIGN DEPTH OF THE TRAP. REMOVED SEDIMENT SHALL BE DEPOSITED IN A SUITABLE AREA AND IN SUCH A MANNER THAT IT WILL NOT ERODE.
  3. INLET PROTECTION SHOULD REMAIN IN PLACE AND OPERATIONAL UNTIL THE DRAINAGE AREA IS COMPLETELY STABILIZED. IMMEDIATELY STABILIZE THE AREA DISTURBED BY THE INSTALLATION AND REMOVAL OF THE PRACTICE.
  4. IT IS ESSENTIAL THAT MAINTENANCE BE DONE TO INSURE THAT STRUCTURES DO NOT FAIL, ESPECIALLY TO PREVENT CLOGGING. FAILURE OF ONE PRACTICE CAN CREATE A DOMINO EFFECT OF FAILURES, WITH THE POTENTIAL OF SEVERE FLOODING OF ADJACENT PROPERTIES.



STABILIZED STONE CONSTRUCTION ENTRANCE  
NO SCALE

- NOTES:**
- (1) WIDTH SHALL EXTEND FULL WIDTH OF INGRESS AND EGRESS OPERATION
  - (2) COURSE AGGREGATE SHALL BE 2-4 INCH STONE FOR LOW VOLUME ENTRANCES OR 4-6 INCH STONE FOR HEAVY USE OR MATERIAL DELIVERY ENTRANCES.
- MAINTENANCE:**
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
  2. WHEELS ON ALL VEHICLES SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCES ONTO PUBLIC RIGHTS-OF-WAY.
  3. INSPECTION AND NEEDED MAINTENANCE SHOULD BE PROVIDED DAILY BUT AT A MINIMUM EVERY SEVEN DAYS AND AFTER EVERY RAIN OF 0.5 INCH OR GREATER.



LEGEND

494 SPRUCE STREET  
FOR  
CAMPUS ACQUISITIONS HOLDINGS, LLC  
MORGANTOWN, WEST VIRGINIA



ALPHA ASSOCIATES, INC.  
209 PRAIRIE AVENUE  
MORGANTOWN, WV 26501  
PHONE/FAX: 304-296-8216  
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www.thinkALPHAfirst.com

REVISIONS

ITEM	DATE

PROJ. NO.: 1310125.01  
DATE: 08/06/2014  
SHEET NO.:

DETAILS

8

ISSUED FOR: SCHEMATIC DESIGN

Drawing Issue	Date
SD Progress Set	2014-01-09
100% Schematic Design	2014-01-24
PCBZA SUBMISSION REVISIONS	2014-04-25
Planning Comm. Submission	2014-08-08

494 Spruce Street  
Morgantown, WV

Client:



Campus Acquisitions Holdings, LLC  
161 N Clark Suite 2050  
Chicago, IL 60601  
312-994-1874

Landscape Architect:

Roofmeadow  
7135 Germantown Avenue  
2nd Flr  
Philadelphia, PA 19119

MEP/FP/IT Engineer:

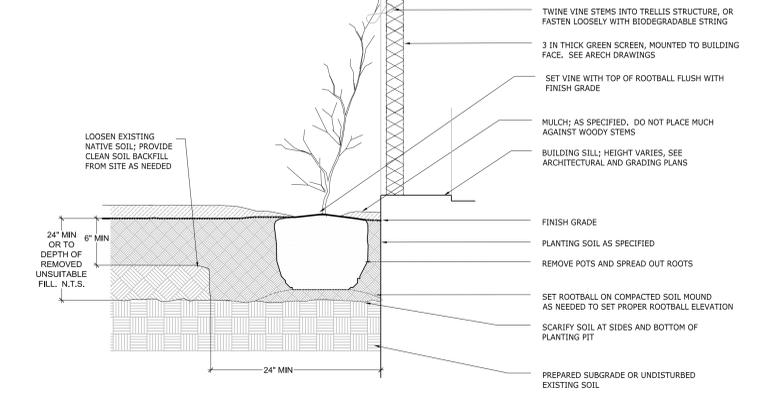
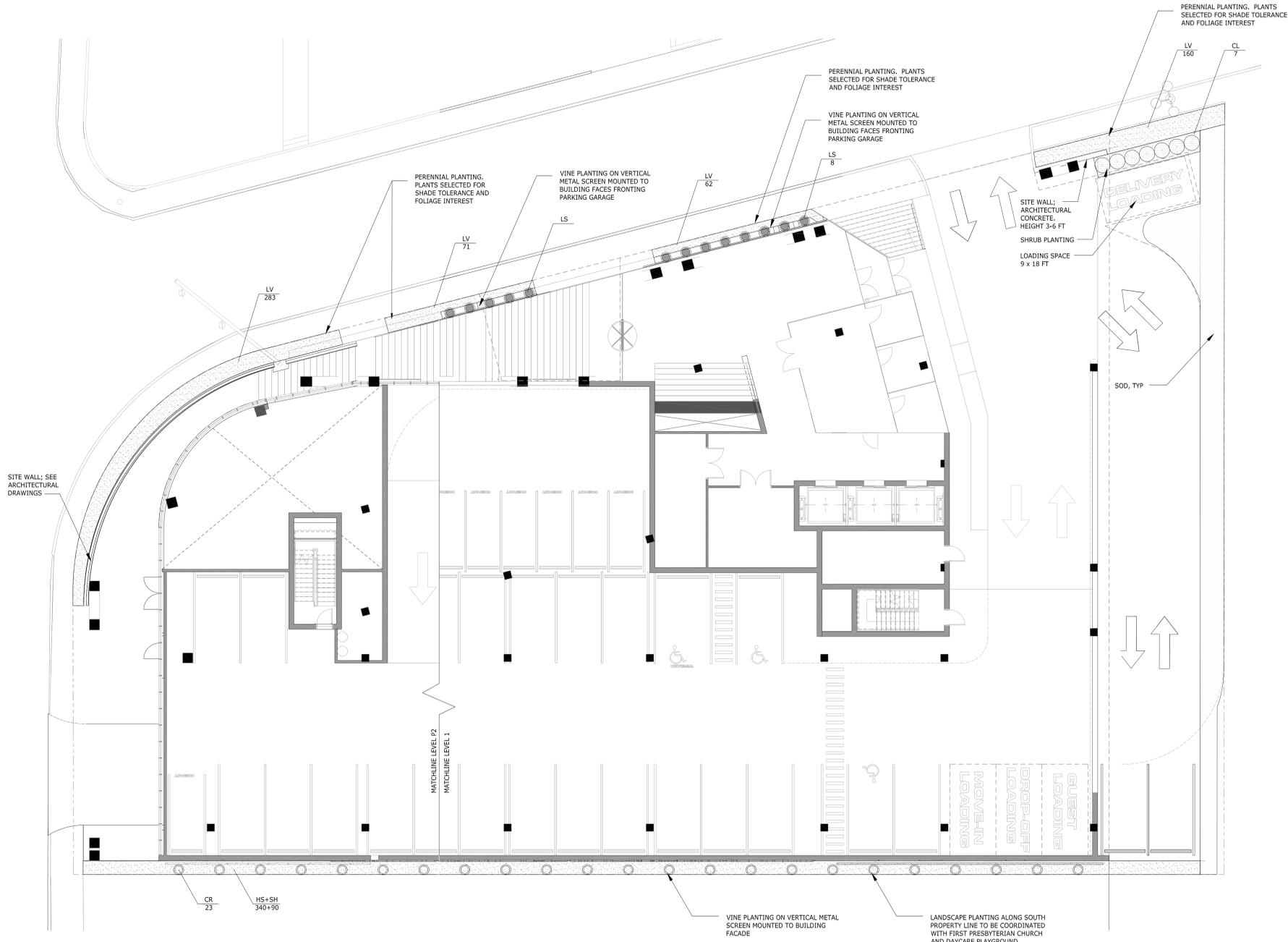
Environetics Design, Inc.  
One Penn Center  
1617 JFK Blvd, Suite 1600  
Philadelphia, PA 19103

Structural Engineer:

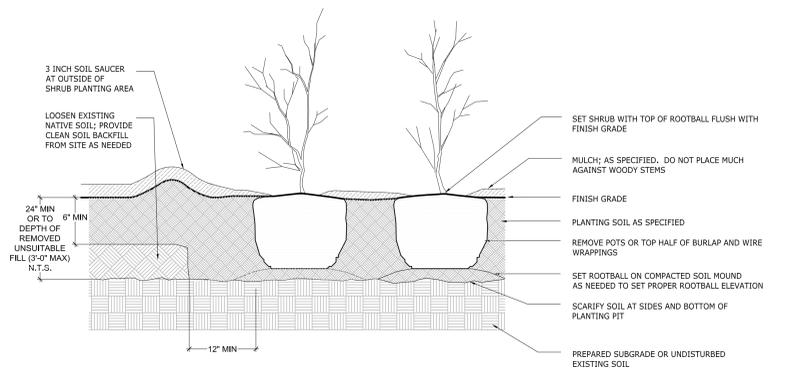
O'Donnell & Naccarato  
111 S. Independence Mall East  
Suite 500  
Philadelphia, PA 19106

Civil Engineer:

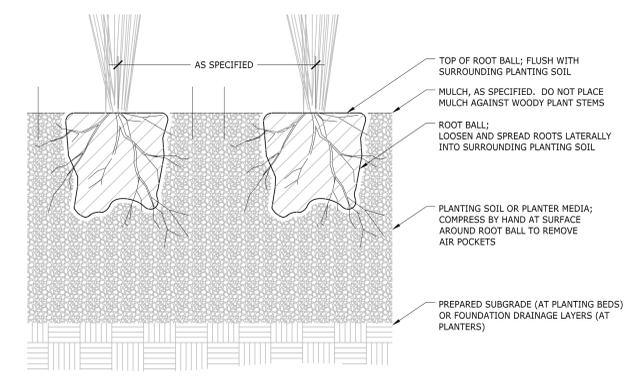
Alpha Associates, Inc.  
209 Prarie Avenue  
Morgantown, WV 26501-5934



1 VINE PLANTING AT BUILDING FACE  
NOT TO SCALE



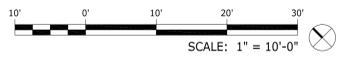
2 TYPICAL SHRUB PLANTING  
NOT TO SCALE



3 TYPICAL PERENNIAL AND CONTAINER PLANTING  
3" = 1'-0"

PLANT SCHEDULE

	SCIENTIFIC NAME	COMMON NAME	QTY	SPACNG	MIN. SIZE	CONDITION	REMARKS
SHRUBS	CL Clethra alnifolia 'Hummingbird'	Summersweet Clethra	7	36" on center	18 in. ht.	Container grown	requires some soil moisture
	Ilex verticillata 'Red Sprite'	Dwarf Winterberry			18 in. ht.	Container grown	alternate species
VINES	CR Campsis radicans	Trumpet Vine	23	48" on center	#2 Container	Container grown	vine for sun, yellow flowered cultivar
	LS Lonicera sempervirens	Trumpet Honeysuckle	13	as noted	#2 Container	Container grown	shade tolerant, twining, semi-evergreen
	Hedera helix	English Ivy			#2 Container	Container grown	alternate for shade, clinging, evergreen
PERENNIALS	LV Liriope muscari 'Variegata'	Variegated Liriope	576	12" on center	#1 Container	Container grown	at front of planting area; shade tolerant
	Heuchera villosa	Alum Root		12" on center	#1 Container	Container grown	alternate species, shade tolerant
	Ophiopogon japonicus	Mondo Grass		10" on center	#1 Container	Container grown	alternate species, shade tolerant
	HS Hemerocallis 'Stella D'Oro'	Daylilies	340	12" on center	#1 Container	Container grown	front of planting area, for sun
	SH Sporobolus heterolepis	Prairie Dropseed	90	24" on center	#1 Container	Container grown	middle of planting area, for sun
SOD			± 755 SF				All remaining landscape areas within the limit of disturbance



NOTES:

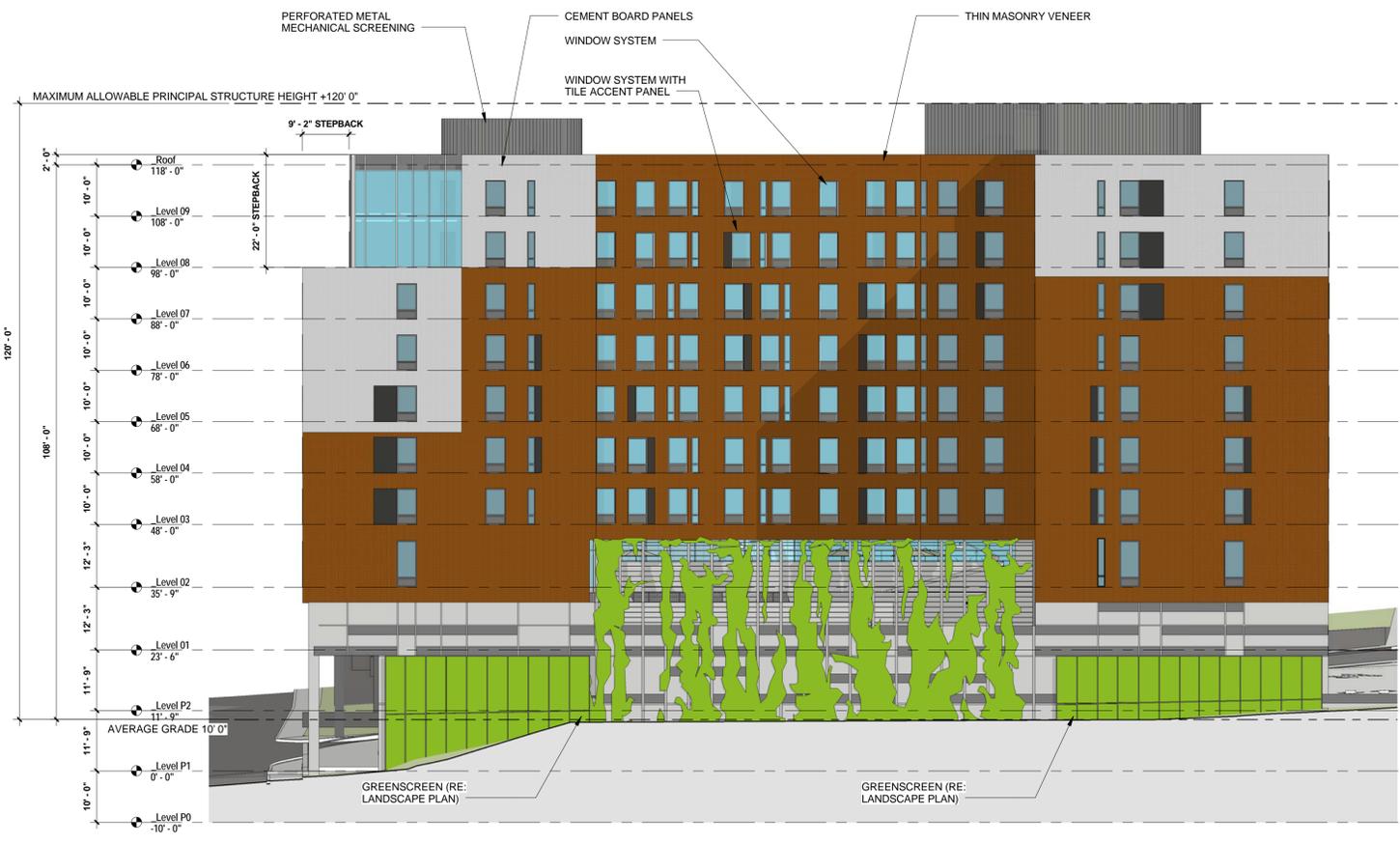
- FOR GRADING AND UTILITY INFORMATION, SEE CIVIL DRAWINGS.
- DECORATIVE METAL SCREEN TO BE MODULAR PANELS OF STAINLESS STEEL OR POWDERCOATED GALVANIZED WIRE WITH A SUPPORTING FRAME, SUITABLE FOR WALL MOUNTING. BY GREEN SCREEN OR SIMILAR.
- MARK UTILITIES IN THE FIELD PRIOR TO PLANTING. ADJUST PLANT LOCATIONS IF NECESSARY IN COORDINATION WITH LANDSCAPE ARCHITECT.
- ALTERNATE PLANT SPECIES ARE PROVIDED TO ACCOMMODATE AVAILABILITY AT TIME OF CONSTRUCTION
- PROVIDE DRIP IRRIGATION FOR NARROW PLANTING AREAS ALONG WILLEY STREET AND FOR ANY PLANTING AREAS BELOW THE BUILDING OVERHANG

Landscape Plan and Details

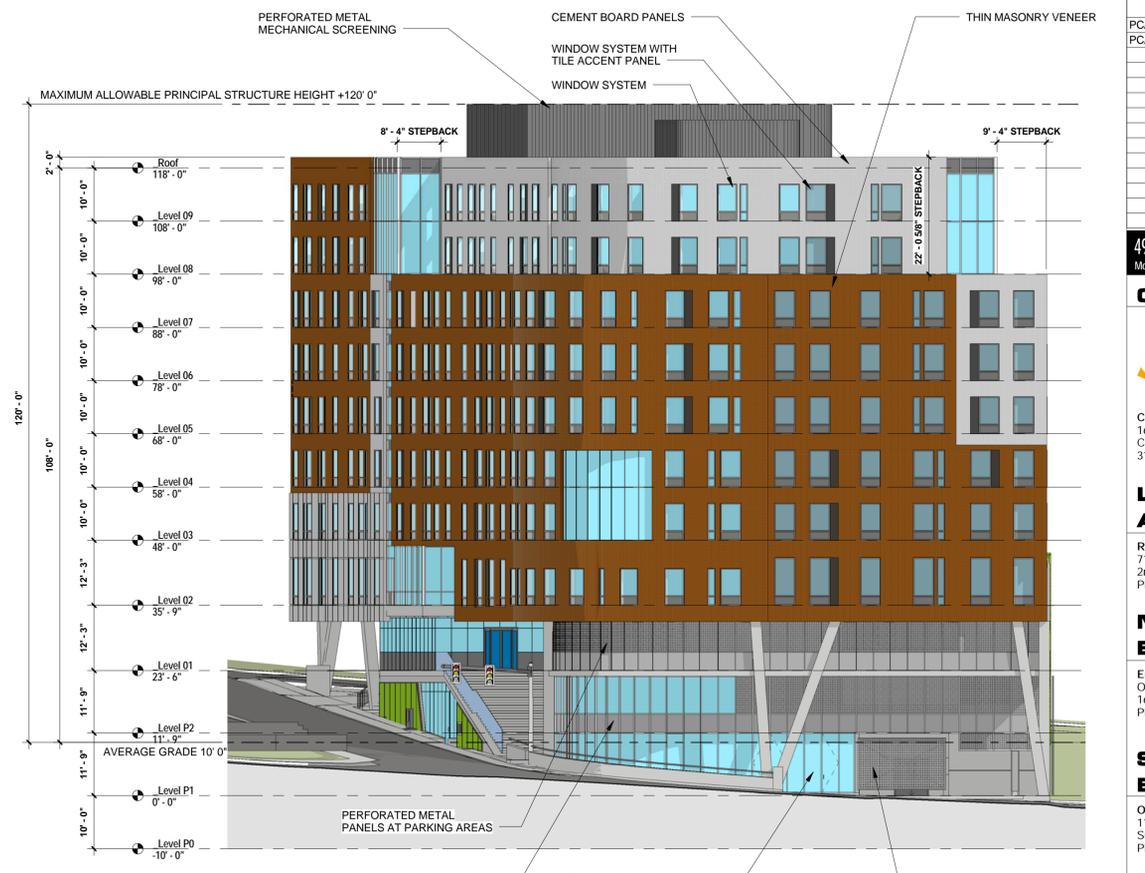
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DRAWN BY: LH	PROJECT NO: 1149.00
DRAWING NUMBER:	



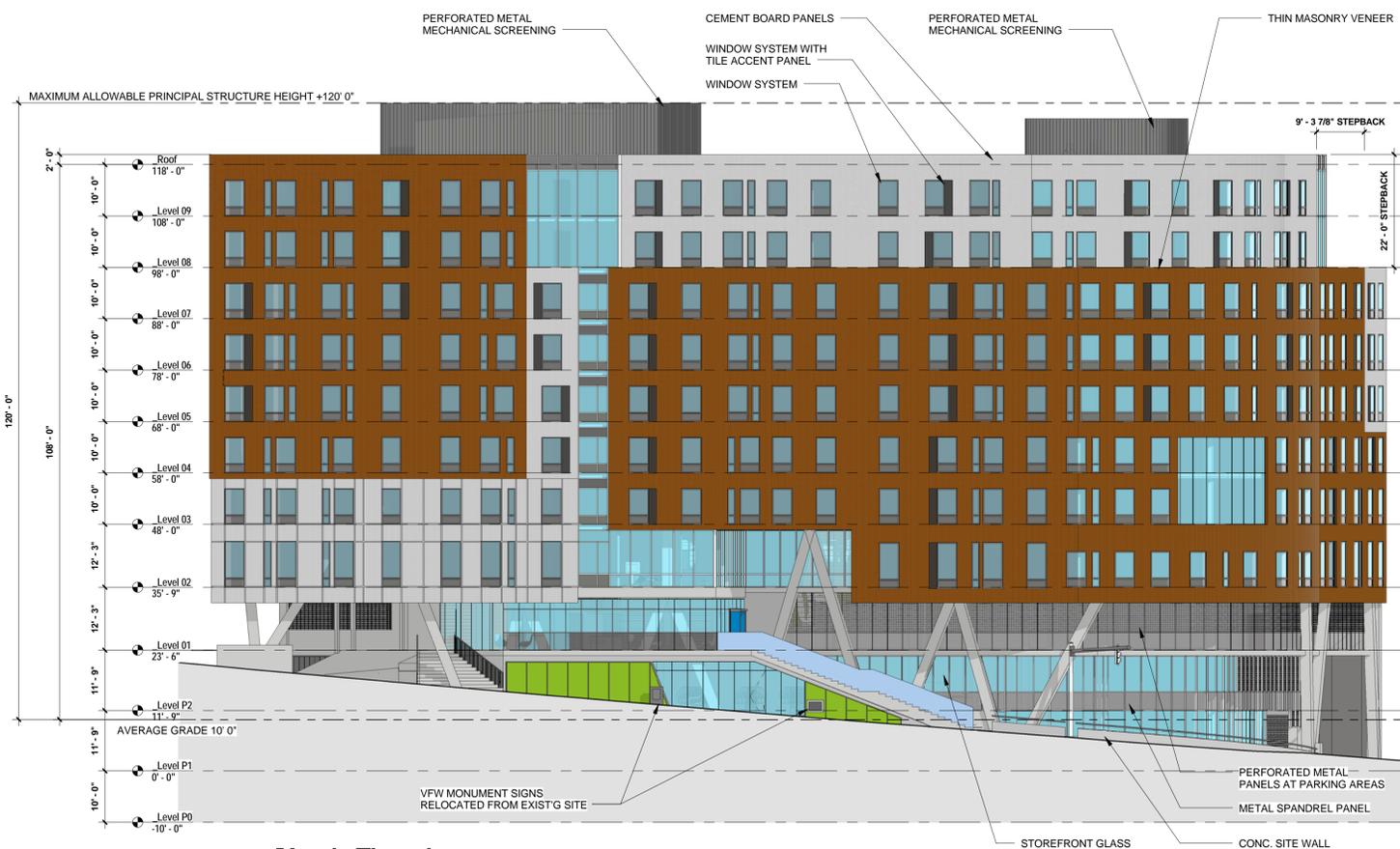




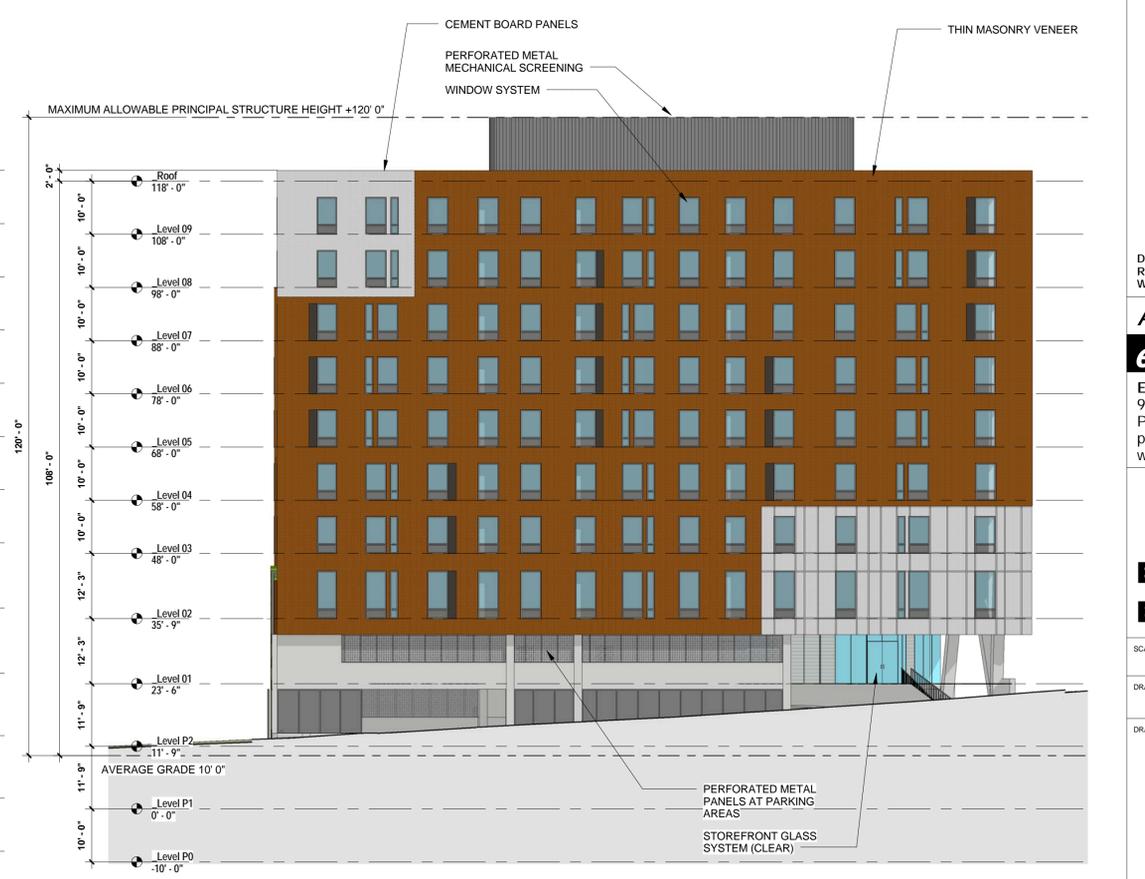
**4 South Elevation**  
Scale: 1/16" = 1'-0"



**2 West Elevation**  
Scale: 1/16" = 1'-0"



**3 North Elevation**  
Scale: 1/16" = 1'-0"



**1 East Building Elevation**  
Scale: 1/16" = 1'-0"

Drawing Issue	Date
PC/BZA SUBMISSION	2014-04-04
PC/BZA SUBMISSION 2	2014-08-08

494 Spruce Street  
Morgantown, WV

**Client:**  
**CA STUDENT LIVING**  
Campus Acquisitions Holdings, LLC  
161 N Clark Suite 2050  
Chicago, IL 60601  
312-994-1874

**Landscape Architect:**  
**RoofMeadow**  
7135 Germantown Avenue  
2nd Flr  
Philadelphia, PA 19119

**MEP/FP/IT Engineer:**  
**Environetics Design, Inc.**  
One Penn Center  
1617 JFK Blvd, Suite 1600  
Philadelphia, PA 19103

**Structural Engineer:**  
**O'Donnell & Naccarato**  
111 S. Independence Mall East  
Suite 950  
Philadelphia, PA 19106

**Civil Engineer**  
**Alpha Associates, Inc.**  
209 Prairie Avenue  
Morgantown, WV 26501-5934

DAVID MCHENRY, AIA LIC. # NO-4615  
REGISTERED ARCHITECT - STATE OF WEST VIRGINIA

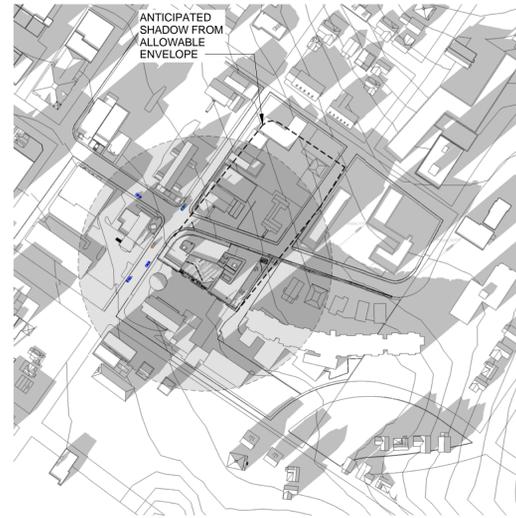
**Architect:** SCOTT A. ERDY, AIA  
DAVID S. MCHENRY, AIA  
**emArchitecture**  
Erdy McHenry Architecture, LLC  
915 North Orianna Street  
Philadelphia, Pennsylvania 19123  
ph: 215.925.7000 fax: 215.925.1990  
web: http://www.em-arc.com

**Building Elevations**

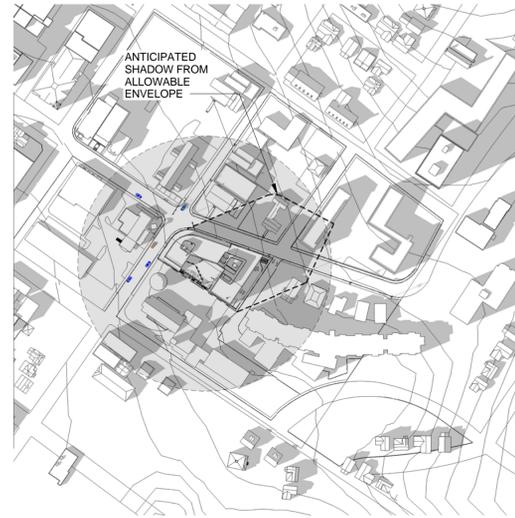
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DRAWN BY: AR / LS	PROJECT NO: 1149.00

DRAWING NUMBER:

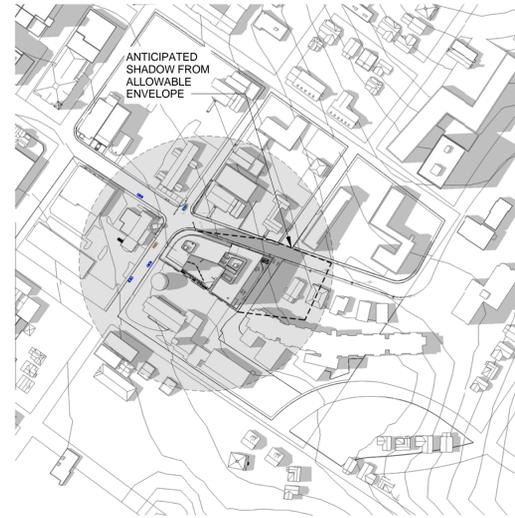




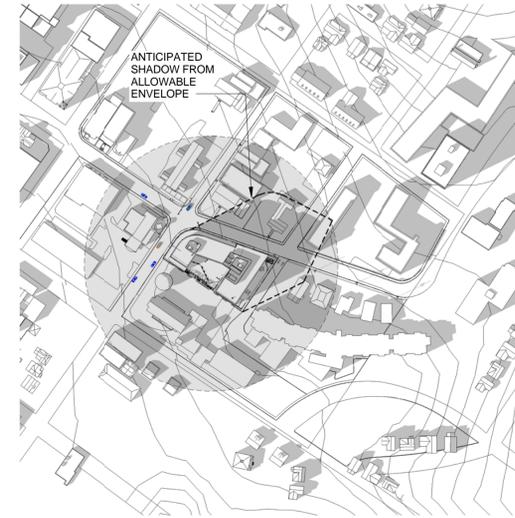
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Scale: 1" = 200'-0"



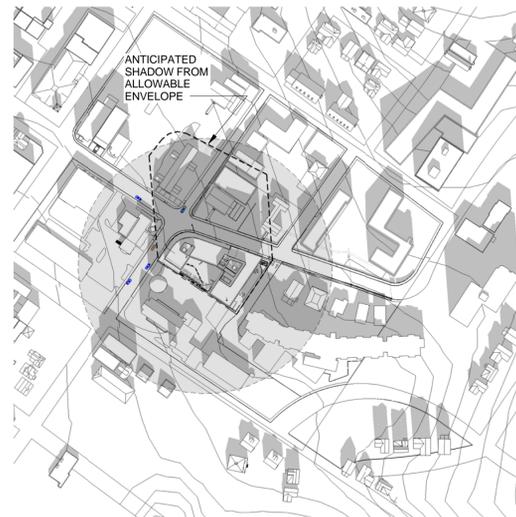
**9 Sun Study\_ Autumn - 4pm**  
Scale: 1" = 200'-0"



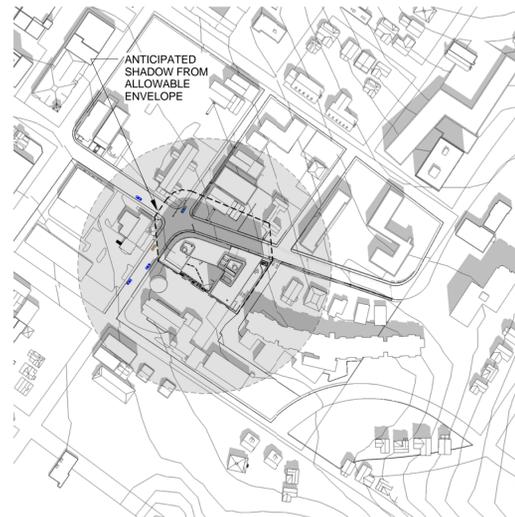
**6 Sun Study\_ Summer - 4pm**  
Scale: 1" = 200'-0"



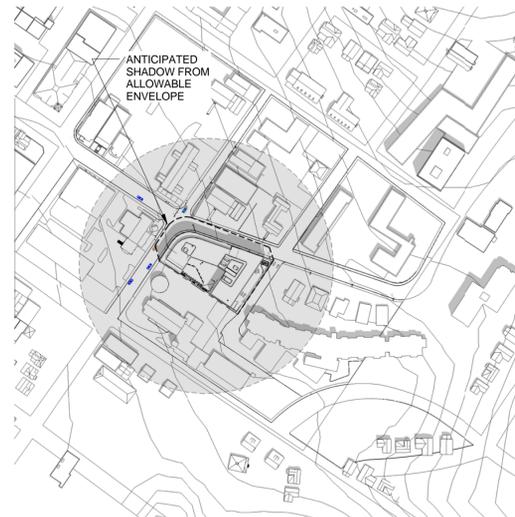
**3 Sun Study\_ Spring - 4pm**  
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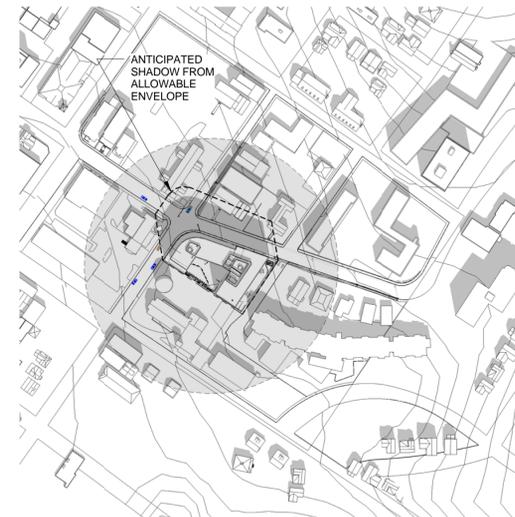
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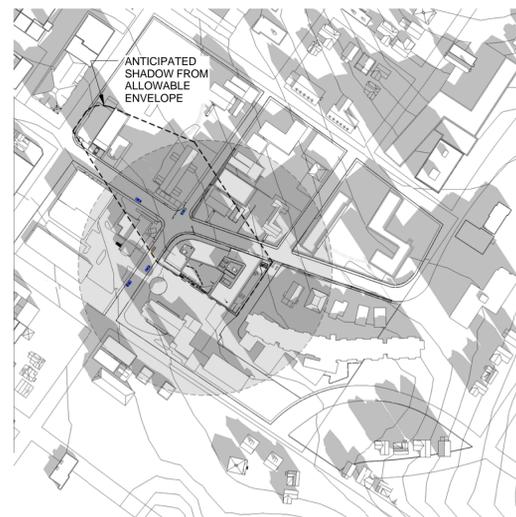
**8 Sun Study\_ Autumn - 12pm**  
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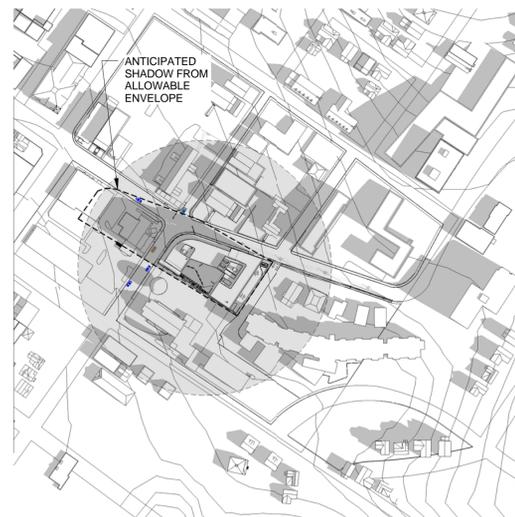
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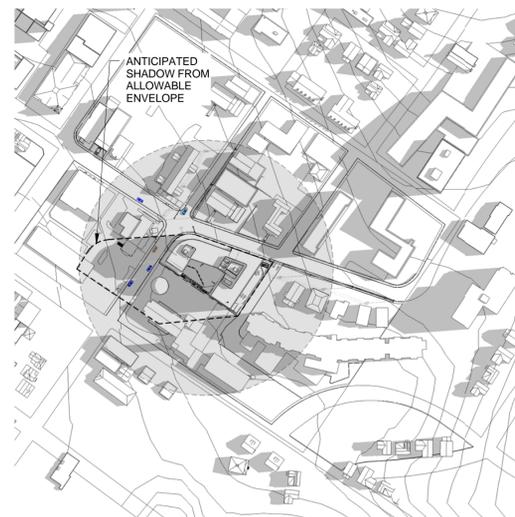
**2 Sun Study\_ Spring - 12pm**  
Scale: 1" = 200'-0"



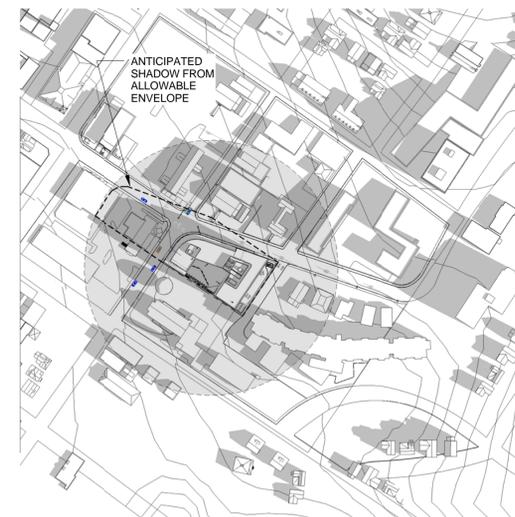
**10 Sun Study\_ Winter - 10am**  
Scale: 1" = 200'-0"



**7 Sun Study\_ Autumn - 9am**  
Scale: 1" = 200'-0"



**4 Sun Study\_ Summer - 8am**  
Scale: 1" = 200'-0"



**1 Sun Study\_ Spring - 9am**  
Scale: 1" = 200'-0"

Drawing Issue	Date
PCBZA SUBMISSION	2014-04-04
PCBZA SUBMISSION REVISIONS	2014-04-25
PCBZA SUBMISSION 2	2014-08-08

494 Spruce Street  
Morgantown, WV

**Client:**  
 **STUDENT LIVING**  
Campus Acquisitions Holdings, LLC  
161 N Clark Suite 2050  
Chicago, IL 60601  
312-994-1874

**Landscape Architect:**  
RoofMeadow  
7135 Germantown Avenue  
2nd Flr  
Philadelphia, PA 19119

**MEP/FP/IT Engineer:**  
Environetics Design, Inc.  
One Penn Center  
1617 JFK Blvd, Suite 1600  
Philadelphia, PA 19103

**Structural Engineer:**  
O'Donnell & Naccarato  
111 S. Independence Mall East  
Suite 950  
Philadelphia, PA 19106

**Civil Engineer**  
Alpha Associates, Inc.  
209 Prarie Avenue  
Morgantown, WV 26501-5934

DAVID MCHENRY, AIA LIC. # NO-4615  
REGISTERED ARCHITECT - STATE OF WEST VIRGINIA

Architect: SCOTT A. ERDY, AIA  
DAVID S. MCHENRY, AIA

**emArchitecture**

Erdy McHenry Architecture, LLC  
915 North Orianna Street  
Philadelphia, Pennsylvania 19123  
ph: 215.925.7000 fax: 215.925.1990  
web: <http://www.em-arc.com>

**Sun Studies**

SCALE: 1" = 200'-0"	DATE: 06/11/2014
DRAWN BY: AR / LS	PROJECT NO: 1149.00
DRAWING NUMBER:	



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494 Spruce Street  
Morgantown, WV 26505

# PEDESTRIAN WIND FLOW ANALYSIS REPORT

Submitted to: The City of Morgantown Planning and Zoning  
389 Spruce Street  
Morgantown, WV 26505



Figure 1 Site Location (source: google map)

Consultant: ISOENV Environmental Design Lab  
2200 Benjamin Franklin Pkwy  
Philadelphia, PA 19130

Jihun Kim, Principal Investigator  
Registered Architect of Pennsylvania [RA405933]

## SUMMARY

The objective of current research is to predict the change in wind condition at the pedestrian level. Computational wind tunnel analysis was conducted to understand impact of the proposed building on wind condition at its neighborhood. Only the immediate surrounding buildings are geometrically modeled because they have the most significant impact on wind flow. The purpose of the analysis is for the city of Morgantown to estimate the microclimate change caused by new construction compared to the existing condition.

Computational wind tunnel analysis has been validated and widely accepted in wind engineering and urban scale environmental analysis, replacing physical wind tunnel test that costs much more resources and time along with its own uncertainties, such as translation issue to real-life size from scale model in the test. The consultant used one of the most advanced wind simulation in the industry for high prediction accuracy: ANSYS Fluent v14. Please note that there will be a simplification process, as the general wind engineering approach, by selectively including geometries that are significant to wind speed and pattern in urban scale. For example, buildings are considered but street lights are not.

The climate data in use is 'Typical Meteorological Year' (TMY), which is available from the National Oceanic and Atmospheric Administration ([www.noaa.gov](http://www.noaa.gov)). We use this data type because it is synthesized with 30 year period weather so that it would better represent the longevity of buildings. It is also generally acceptable in wind engineering and urban climate analysis. 'Morgantown Municipal Airport' is chosen, given it is the nearest available TMY data that is only 2.5 miles away from the site of interest.

As results, our statistical analyses with the simulation outcome showed the minor impact on pedestrian wind condition. Average 0.96% of wind velocity is reduced at +2m above ground of the entire neighborhood, as it is shown at Table 1. It is because larger foot print of the building allowed less wind on the narrower street so that more wind was pushed to the atmosphere, when comparing Figure 3 to Figure 4. Another reason is 'wind shade effect', which reduced overall wind speed behind the proposed building that is taller than the existing, comparing Figure 5 to 6. We came to conclude that the proposed building may reduce pedestrian wind speed with minor degree based on our prediction result.

**Table 1 Wind speed analysis at pedestrian level**

	maximum	minimum	average	median
proposed	5.39	0.00	1.29	0.98
existing	4.62	0.00	1.32	1.05

# REGIONAL WIND ANALYSIS

Annual wind condition of Morgantown is graphically represented at Figure 2, which shows more than 35 % of time wind comes from southwest with average speed of 4.5 m/s. Therefore, with this high frequency, this condition constitutes prevailing wind, which will be used as the input values for wind flow analysis.

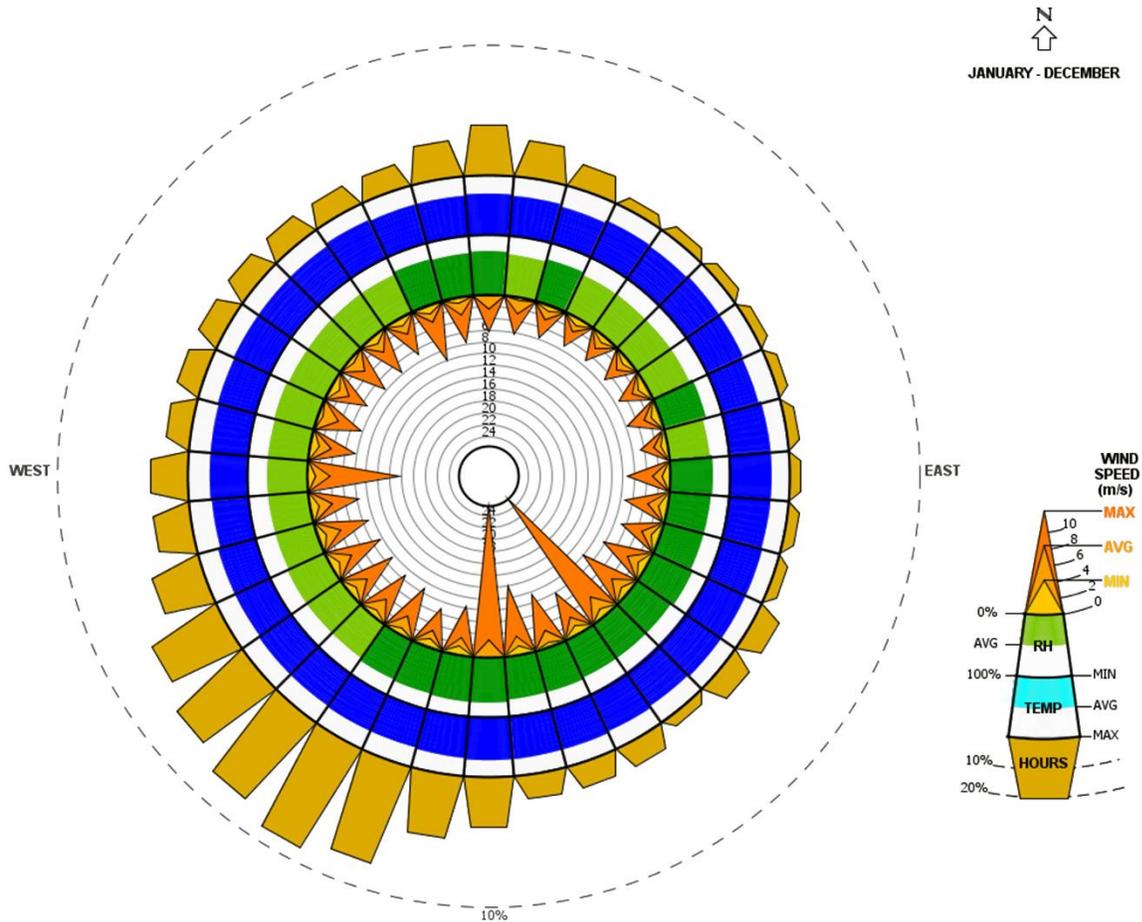
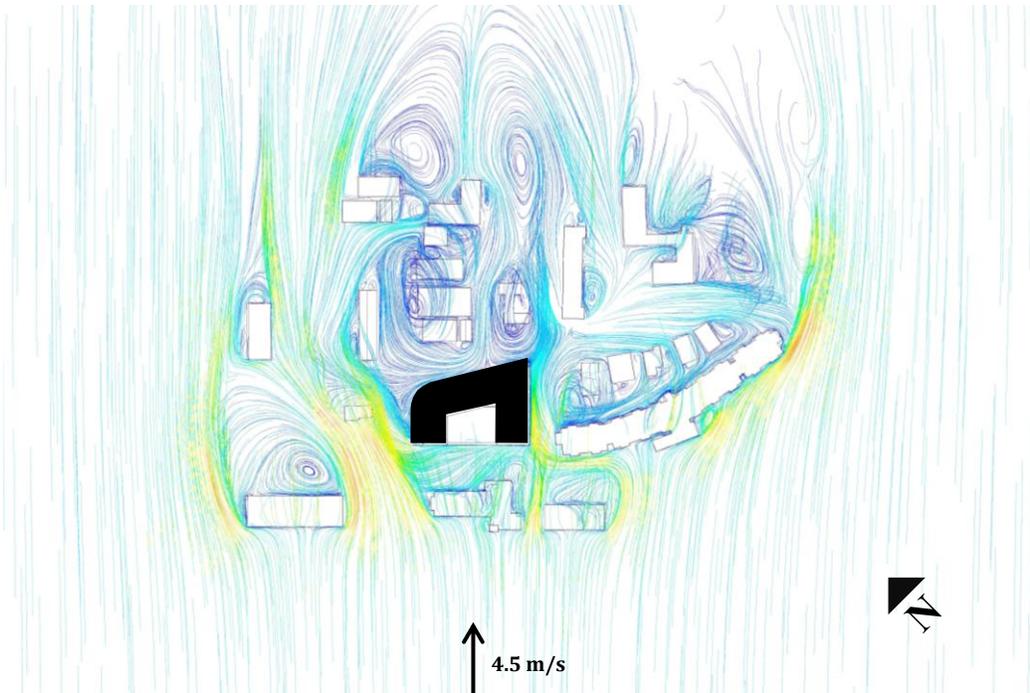
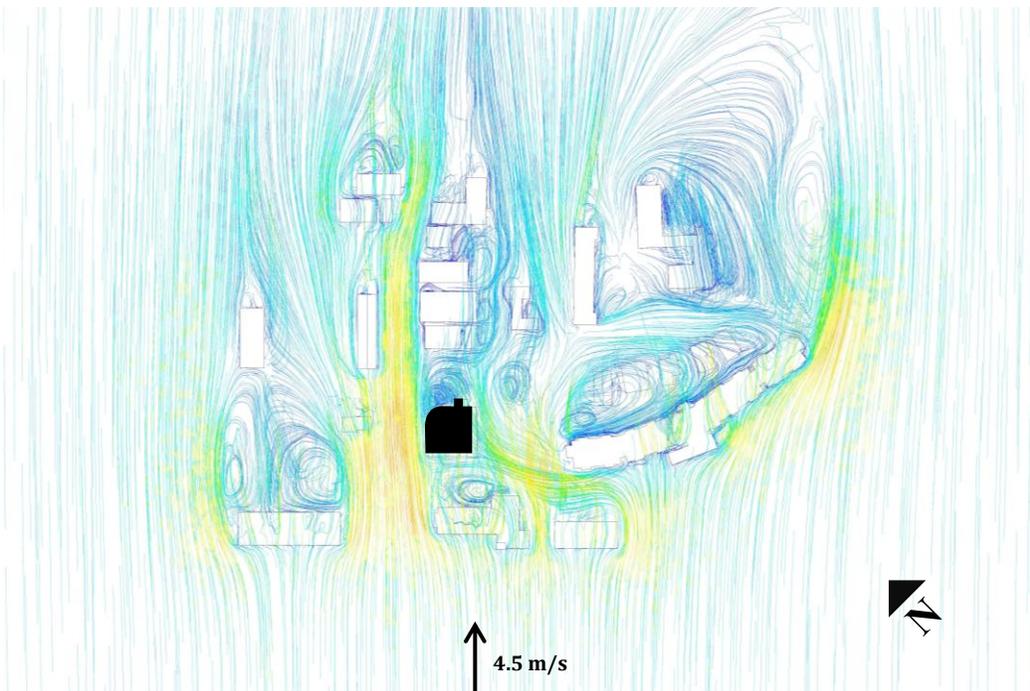


Figure 2 Annual Wind Rose



**Figure 3 Proposed Building in Dark Shade - Wind Path lines at Pedestrian Level at +2m \***



**Figure 4 Existing Building in Dark Shade - Wind Path lines at Pedestrian Level at +2m \***



\* Color represents wind velocity and path lines represent the track of wind flow.

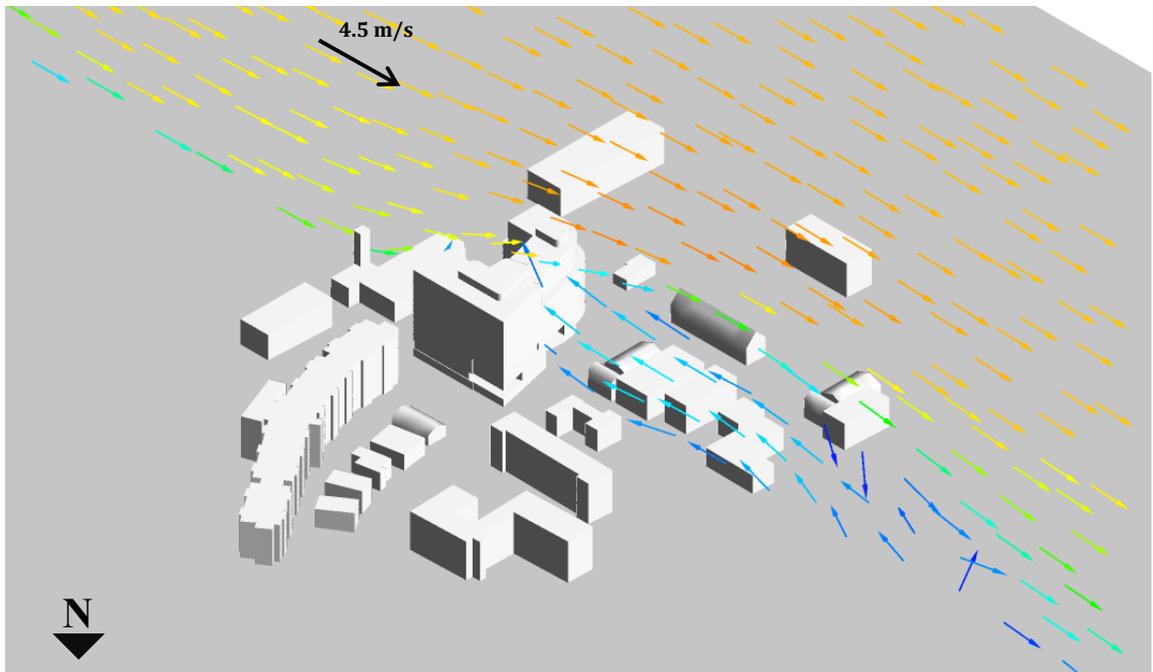


Figure 5 Proposed - Vector on Vertical Plane \*\*

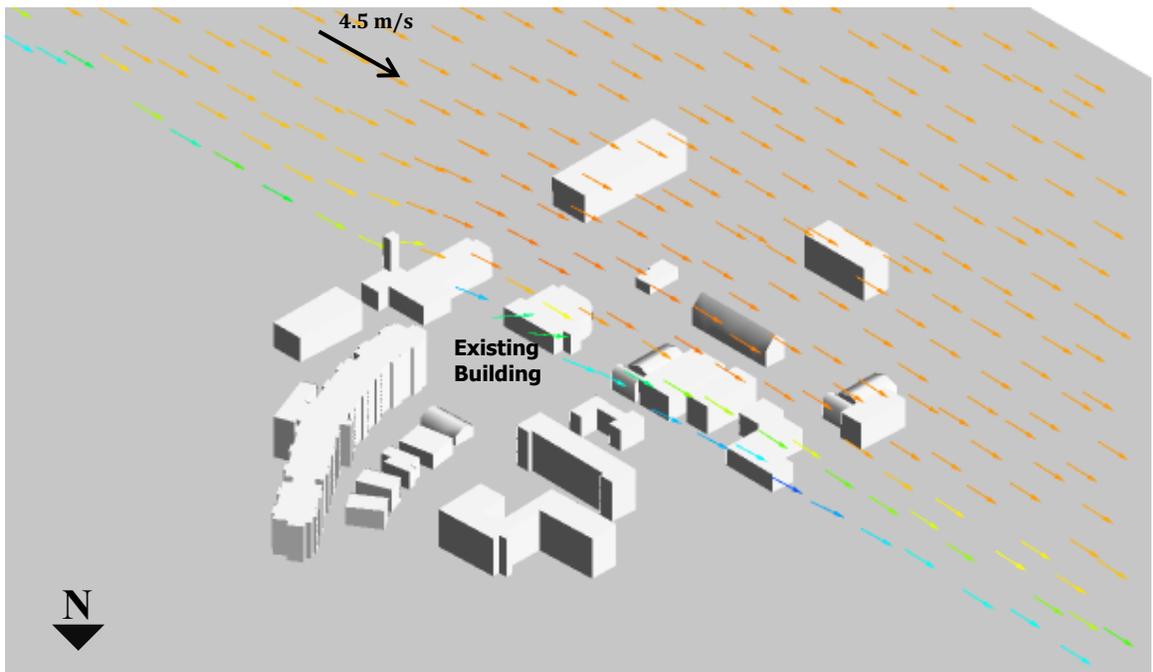
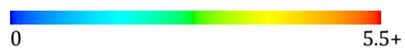
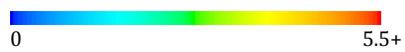


Figure 6 Existing - Vector on Vertical Plane \*\*



\*\* Arrow represents direction and color represents velocity

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Excellence Delivered **As Promised**

August 7, 2014

Mr. Dan Hrankowsky  
Director of Design  
CA Student Living  
161 N Clark Street, Suite 2050  
Chicago, IL 60601

**Re: Proposed 494 Spruce  
Residential/Commercial Development**

Dear Mr. Hrankowsky:

Gannett Fleming, Inc. recently completed a Traffic Impact Study for the above-captioned project dated May 28, 2014. At the time, the report addressed a mixed-use development containing 368 bedrooms and 3,500 square feet of commercial space. Since that time, the development proposal has been modified to include 331 bedroom with the same amount of commercial space.

As indicated in the study, the anticipated trip generation for the residential portion of the development was based on research conducted at similar facilities. This research included the development of rates based on the number of apartment units, number of bedrooms, and number of parking spaces. The rates utilizing the number of bedrooms as the independent variable yielded the most conservative results and were therefore utilized for the analysis. This trip generation methodology was accepted by the West Virginia Division of Highways.

Considering the proposed change in residential intensity, the following tables outline a comparison of the anticipated trip generation for the original proposal versus the revised proposal.

Original Trip Generation Estimates

Land Use	AM Peak Hour			PM Peak Hour			Daily		
	In	Out	Total	In	Out	Total	In	Out	Total
368 Bedrooms	11	15	26	25	23	48	261	262	523
3,500 SF Retail	12	12	24	10	8	18	78	77	155
<b>Total</b>	<b>23</b>	<b>27</b>	<b>50</b>	<b>35</b>	<b>31</b>	<b>66</b>	<b>339</b>	<b>339</b>	<b>678</b>

**Gannett Fleming**

Proposed 494 Spruce

Residential/Commercial Development

Page 2 of 2

**Revised Trip Generation Estimates**

Land Use	AM Peak Hour			PM Peak Hour			Daily		
	In	Out	Total	In	Out	Total	In	Out	Total
331 Bedrooms	10	13	23	23	20	43	235	235	470
3,500 SF Retail	12	12	24	10	8	18	78	77	155
<b>Total</b>	<b>22</b>	<b>25</b>	<b>47</b>	<b>33</b>	<b>28</b>	<b>61</b>	<b>313</b>	<b>312</b>	<b>625</b>

As can be seen by comparing the tables, there is an 11 to 13 percent reduction in the number of trips for the residential portion of the development based on the revised development proposal.

The Traffic Impact Study summarized that the proposed development would have a minor impact on the adjacent roadway system, and as such no roadway, traffic signal, or other system improvements were recommended. It is our determination based on the above information that the revised proposal will have less of an impact to the adjacent roadway system as compared to the original proposal, and therefore the study outcome has not changed.

If you have any questions regarding this information, please do not hesitate to contact me.

Very truly yours,

**Gannett Fleming, Inc.**



Mark Metil, P.E., PTOE

Director of Transportation Operations and Planning

***The subject Traffic Impact Study dated  
May 28, 2014 was provided to the Planning  
Commission for the June 12, 2014 hearing  
and should be considered a part of the  
record for Case No. S14-07-III***

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WEST VIRGINIA DEPARTMENT OF TRANSPORTATION

**Division of Highways**

1900 Kanawha Boulevard East • Building Five • Room 110  
Charleston, West Virginia 25305-0430 • (304) 558-3505

Earl Ray Tomblin  
Governor

Paul A. Mattox, Jr., P. E.  
Secretary of Transportation/  
Commissioner of Highways

August 8, 2014

Mr. Dan Hrankowsky  
Director of Design  
CA Student Living  
161 N Clark  
Suite 2050  
Chicago, Illinois 60601

Dear Mr. Hrankowsky:

In June 2014, the West Virginia Division of Highways (WVDOH) issued approval of a Traffic Impact Study (TIS) regarding the proposed 494 Spruce Development to be located adjacent to US 119 in Morgantown, Monongalia County. You recently notified the WVDOH that certain revisions to the scope of the project are proposed, and the WVDOH has reviewed the revised information presented to us.

The results of our review indicate that based on the information provided, the WVDOH sees no need for you to provide the WVDOH with any updates to the previously approved TIS, as the revisions are relatively negligible and no increase in trip generation is proposed. The previously approved TIS still would be in effect for WVDOH permitting purposes with no revisions necessary and the TIS would be finalized after you have addressed appropriately any comments/concerns you may receive from the City of Morgantown and/or the Greater Morgantown Metropolitan Planning Organization. As noted previously, you would need to provide the WVDOH with three printed versions and two electronic versions of the full, final, approved TIS.

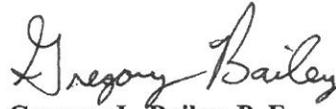
The access location along US 119 (Willey Street) appears to be the same as previously proposed and the WVDOH still is agreeable to that location, provided it has the same restriction (no left turn onto Willey Street from the site) as previously agreed. The information provided to the WVDOH also appears to indicate that the US 119 Northbound (Spruce Street) access now is proposed to be egress only from the site; the WVDOH has no objection to that but we are also still agreeable to a right-in/right-out access at Spruce Street as previously proposed. Also, please provide additional information concerning your proposed Option 1. Our understanding previously was that the bottom level of parking within the site was to be accessible only from Spruce Street. If the Spruce Street access is to be egress only, will the lower level of parking remain and if so, will it now be accessible from Willey Street?

**Mr. Dan Hrankowsky**  
**August 8, 2014**  
**Page Two**

The recommendations and conclusions of the TIS, once finalized, are to be incorporated into the Plans. When desired, you may submit to the WVDOH four sets of the Project Plans and any associated drainage calculations for review by WVDOH as part of our Plan approval/permitting process.

Thank you for your assistance with this matter. Should you require additional information, please contact Mr. David E. Cramer, P. E., of our Commissioner's Office of Economic Development, at (304) 558-9211.

Very truly yours,



**Gregory L. Bailey, P. E.**  
**State Highway Engineer**

**GLB:Cb**

**cc: Mr. Damien Davis, City of Morgantown**  
**Mr. Chris Fletcher, City of Morgantown**

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**Witt Economics LLC  
P.O. Box 18146  
Morgantown WV 26507**

September 3, 2014

Dan Hrankowsky  
Director of Design  
CA Student Living  
161 North Clark Suite 4900  
Chicago IL 60601

Dear Dan,

The following information is provided as a supplement to my report, *The Economic Impact of Proposed Student Housing on the City of Morgantown and Monongalia County*, which was discussed at the Morgantown Planning Commission on June 12, 2014.

Attached you will find an abbreviated curriculum vitae. I have been actively involved in economic development research and service since I arrived at West Virginia University in 1970. Some key highlights include:

- Served as a principal investigator or co-PI on over \$6 million in sponsored research and contracts with organizations. Examples include the U.S. Environmental Protection Agency, U.S. Department of Justice, West Virginia Legislature, West Virginia Development Office, West Virginia Division of Energy, The Greenbrier Resort, West Virginia Oil and Gas Association, and the Claude Worthington Benedum Foundation, among others.
- As director of the WVU Bureau of Business and Economic Research, I directed a team of MA and Ph.D. economists focused on economic development within West Virginia. We issued regular economic and demographic forecasts, conducted industry and labor market studies, researched state and local public finance issues, and conducted public policy research. The BBER website [www.bber.wvu.edu](http://www.bber.wvu.edu) provides additional information about our past research publications.
- The Monongalia County Commission appointed me to the Monongalia County Development Authority in 1993, where I served until 2009. From 1996-2009 I was the vice president of the authority.
- Since retiring from WVU I formed Witt Economics LLC to provide economic research and analysis to a variety of organizations. I have used IMPLAN to conduct economic impact studies for Braskem America, West Virginia Wesleyan College, West Virginia School of Osteopathic Medicine, Camp

Caesar (Webster County), and CA Student Living. The study funded by Braskem America, *Building Value from Shale Gas: The Promise of Expanding Petrochemicals in West Virginia*, has been widely quoted and used to convey the potential associated with construction and operation of a world class ethane cracker, associated polyethylene plants and downstream plastics manufacturing within West Virginia.

- I have provided expert witness testimony on a variety of cases before the West Virginia Public Service Commission. Several of these cases involved research reports prepared using IMPLAN.

I understand that Planning Commission members raised questions about the students' expenditure in downtown Morgantown. The significant economic impacts associated with building occupancy reported in the tables excluded expenditures by tenants in downtown Morgantown. As I indicated in the report WVU provides estimates of the estimated costs of attendance expected by student seeking financial aid. These estimates include nearly \$2,000 per student (and as much as \$3,000 or more) for miscellaneous living expenses.

Because of this omission I stated that in a given year the building occupants could add upwards of nearly \$700,000 in spending in retail stores, restaurants, food stores, entertainment, book stores, drug stores, beauty and hair salons, etc., much of which will be in the downtown due to the proximity of apartments to the business community. An examination of Main Street Morgantown businesses shows that many of these businesses cater to students as well as the general population. Thus this development, along with other developments in the Morgantown urban core, should increase the commercial viability of locally owned small business.

Additional questions have been raised about the use of IMPLAN in the calculations of the economic impacts associated with the construction and operation of the project. Central to IMPLAN is an input-output model with upwards of 420 industry sectors. The computational software permits a trained analyst to determine the economic impact of new businesses, business expansion/contraction, and closures. The IMPLAN system is used by numerous federal, state and local agencies as well as private businesses and economic development organizations. Further information can be found at [www.implan.com](http://www.implan.com).

I regret not being able to attend to answer questions but a previous scheduled out of state trip precludes my attendance.

Yours truly,



Tom S. Witt, Ph.D.  
Managing Director and Chief Economist

## Curriculum Vitae (Abbreviated)

Tom S. Witt, Ph.D.  
Managing Director and Chief Economist  
Witt Economics LLC  
Office: P.O. Box 18146  
Morgantown WV 26508

- Education:** Ph.D. Economics, Washington University (St. Louis), 1974  
MA Economics, Washington University (St. Louis), 1968  
BA Economics, Oklahoma State University, 1966
- Work Experience:** College of Business and Economics, West Virginia University  
Professor of Economics Emeritus, Awarded in 2012 Upon Retirement From WVU  
Associate Dean for Research and Outreach, 1994-2006, 2008-2012  
Director, Bureau of Business and Economic Research, 1986-2012  
Acting Associate Dean, 1985-1986  
Acting Director, Bureau of Business Research, 1985  
Professor of Economics, 1980-2012  
Associate Professor of Economics, 1975-80  
Assistant Professor of Economics, 1970-75  
Graduate School, West Virginia University, Acting Assistant Dean, 1977-78

**Professional Memberships** American Economic Association

### Witt Economics LLC Publications

- Economic Impact of Moundsville Power on the West Virginia Economy*, August 2014  
*The Economic Impact of Proposed Student Housing on the City of Morgantown and Monongalia County*, June 2014  
*Building Value From Shale Gas: The Promise of Expanding Petrochemicals in West Virginia*, December 2013  
*Economic Contributions of Camp Caesar on Webster County and West Virginia 2011 and 2012*, November 2013  
*Economic Impact of West Virginia Wesleyan College FY2012*, March 2013  
*Economic Impact of the West Virginia School of Osteopathic Medicine FY2012*, March 2013

### Selected Recent Scholarly Publications

- Kobus, H., Houck, M., Speaker, P., Riley, R., Witt, T. (2011). Managing Performance in the Forensic Sciences – Expectations in Light of Limited Budgets. *Forensic Science Policy & Management*, 2(2), 36 - 43.
- Houck, M., Riley, R., Speaker, P., Witt, T. (2009). FORESIGHT: A Business Approach to Improving Forensic Science Services. *Forensic Science Policy & Management* 1(2), 85-95.  
<http://www.tandf.co.uk/journals>
- Witt, T. (2011). The Natural Gas Industry's Role in West Virginia's Economic Development. *Views and Visions*-publication of Bowles Rice McDavid Graff & Love LLP. (Spring 2011), 14-15.
- Higginbotham, A., Sen, A., Gurley - Calvez, T., Witt, T. (2008). Teacher Shortages: National and Regional Perspectives. *West Virginia Business and Economic Review*, 15.
- Witt, T. (2004). Does Manufacturing Have A Future in West Virginia? *Capacity*.

Witt, T., Bowen, E., Manzi, P. and Meinert, T. (2012). Fossil Energy Opportunities for West Virginia. West Virginia Division of Energy.

Witt, T., Higginbotham, A., Christiadi, C., Meinert, T., Davis, A., Bison-Huckaby, M. (2011) Braxton County Educational Needs Analysis. Morgantown WV: WVU Bureau of Business and Economic Research.

Witt, T., Godfrey, A. H., Meinert, T. (2011). The Economic Impact of the Charles Town Thoroughbred Horse Racing Industry on the Jefferson County and West Virginia Economies 2010. Morgantown WV: WVU Bureau of Business and Economic Research. [www.bber.wvu.edu](http://www.bber.wvu.edu)

Higginbotham, A., Pellillo, A., Gurley - Calvez, T., Witt, T. (2010). The Economic Impact of the Natural Gas Industry and the Marcellus Shale Development in West Virginia in 2009. Morgantown WV: Bureau of Business and Economic Research. [www.bber.wvu.edu](http://www.bber.wvu.edu)

Higginbotham, A., Pennington, E., Christiadi, C., Witt, T. (2010). Economic Impact of West Virginia Higher Education Institutions FY2008 (pp. 87). Morgantown WV: Bureau of Business and Economic Research. [www.bber.wvu.edu](http://www.bber.wvu.edu)

Witt, T. (2010). Financing West Virginia's Highways: Challenges and Opportunities. Morgantown, WV: Bureau of Business and Economic Research, West Virginia University. [www.bber.wvu.edu](http://www.bber.wvu.edu)

Witt, T. (2009). West Virginia Wage Survey 2008 and West Virginia University vs. Other Employers Wage Survey 2008. WVU Bureau of Business and Economic Research.

Witt, T., Gregory, A. C. (2008). An Economic Profile of the Biosciences Industry in West Virginia. Morgantown WV: Bureau of Business and Economic Research, West Virginia University. [www.bber.wvu.edu](http://www.bber.wvu.edu)

Higginbotham, A., Witt, T., Gurley - Calvez, T. (2008). Teacher Shortages: National and Regional Perspectives. West Virginia Business and Economic Review, Bureau of Business and Economic Research, West Virginia University. [www.bber.wvu.edu](http://www.bber.wvu.edu).

Higginbotham, A., Gurley - Calvez, T., Sen, A., Witt, T. (2007). Cost Differences Among School Districts: The Issue of Student Density. Bureau of Business and Economic Research, West Virginia University. [www.bber.wvu.edu](http://www.bber.wvu.edu).

Higginbotham, A., Witt, T., Gurley - Calvez, T., Sen, A. (2007). Teacher Shortages: National and Regional Perspectives. Bureau of Business and Economic Research, West Virginia University. [www.bber.wvu.edu](http://www.bber.wvu.edu).

Witt, T., Leguizamon, S. (2007). Tourism and the West Virginia Economy. Morgantown WV: Bureau of Business and Economic Research, West Virginia University. [www.bber.wvu.edu](http://www.bber.wvu.edu).

Witt, T. (2007). Financing West Virginia's Highways: An Update. Morgantown, WV: Bureau of Business and Economic Research, West Virginia University. [www.bber.wvu.edu](http://www.bber.wvu.edu).

### **Research Grants and Contracts**

Dr. Witt has served as project director, principal investigator or co-principal investigator on over \$6 million dollars of grants and contracts while at West Virginia University. Examples of awarding institutions include U.S. Environmental Protection Agency, U.S. Department of Justice, West Virginia Legislature, West Virginia Department of Transportation, West Virginia Division of Energy, West Virginia Governors Office, West Virginia Oil and Natural Gas Association, West Virginia Coal Association, Claude Worthington Benedum Foundation, and many others.

### **Recent Professional and Public Service**

Member, Blue Ribbon Commission on Highways, (2012-present).  
Member, West Virginia Tax Modernization, Charleston WV. (2009 - 2011).  
Member, West Virginia Interagency Housing Council, Charleston, WV. (2008 - 2011).  
Appointed Representative from the Association for University Business and Economic Research, Council of Professional Associations on Federal Statistics, Washington, DC. (1989 - 2011).  
Member, Governor's Unemployment Solutions Task Force, Charleston, WV. (2008 - 2009).  
Member, Higher Education Policy Commission Statewide Master Plan Taskforce, Charleston, WV. (2006 -2007).  
Member, Labor Market Information Work Team, Governor's Workforce Development Division, West Virginia Development Office. (2002 - 2003).  
Member, Monongalia County Development Authority, 1993-2009. Served as vice president 1996-2009.

### **Expert Witness Presentations to the West Virginia Public Service Commission**

Moundsville Power LLC (case 14-1221-E-CS)  
Hope Gas, Inc., dba Dominion Hope (case 11-1263-G-PC)  
PATH West Virginia Transmission Company, LLC, et.al. (case 09-0770-E-CN)  
Trans-Allegheny Interstate Line Company (case 07-0508-E-CN)  
West Virginia-American Water Company and Thames Water Aqua Holdings GMBH (case 01-1691-W-PC)

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# The Economic Impact of Proposed Student Housing on the City of Morgantown and Monongalia County

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June 5, 2014

Prepared by

Tom S. Witt, Ph.D.  
Managing Director and Chief Economist  
Witt Economics LLC  
P.O. Box 18146  
Morgantown WV 26507  
304.376.9827  
[tomswitt@gmail.com](mailto:tomswitt@gmail.com)

## Introduction and Overview

In conjunction with this project, CA Student Living (hereafter CASL) has commissioned Witt Economics LLC to estimate the economic impacts of their proposed multistory student housing in downtown Morgantown. This report quantifies the economic impacts on the City of Morgantown budget as well as the Monongalia County economy.<sup>1</sup>

## Project Description

CASL has proposed construction and operation of a mixed-use, eleven-story building located at 494 Spruce Street in downtown Morgantown. The project site is owned and occupied by Veterans of Foreign Wars Post 548 and has been actively marketed for sale for the past 5-6 years. After redevelopment the new building will contain 92 four-bedroom units and 7,104 square feet of nonresidential space. Based on the plan currently before the Planning Commission, it is assumed that about 3,200+/- square feet of the total non-residential space will be leased for retail use.

This study used CASL project information on construction and operating costs to develop the economic impacts. All costs and estimated economic impacts are in terms of 2014 dollars. Construction costs are estimated at \$19 million exclusive of financing. Over the 20-month construction period 175,000 man-hours of labor will be used, averaging around 8,750 man-hours per month, which could be represented during the construction period at two different points as follows:

- In the early phase “Core & Shell” construction period, the work force is comprised of concrete workers, iron workers, plumbers, excavators, and other support trades
- In the later phase “Interiors” construction period, note the work force is comprised of framers, carpenters, electricians, plumbers, HVAC, and a host of critical support trades.

During the construction period the City of Morgantown will receive \$420,000 in business and occupation taxes and approximately \$120,000 in construction fees, including permits and licenses. ***The City of Morgantown is projected to receive a total of \$540,000 in revenues during the construction period.***

---

<sup>1</sup> The estimated taxes are based upon current rates within Monongalia County and the City of Morgantown. The City has submitted an application to the Municipal Home Rule Program for revisions to its finance options. While the current business and occupation tax would be reduced for retail, manufacturing, amusement and wholesale categories, it would be replaced by a municipal consumer sales and use tax. This report excludes any consideration of the consequences of this change on this specific project.

During its first year of operation building management will incur an annual payroll of \$210,000, general utilities of \$165,000 and rental unit utilities of \$165,000. Some of these utilities will be for water and sewage services provided by the Morgantown Utility Board. The City of Morgantown is projected to receive \$32,000 in business and occupation taxes and \$18,400 in fire service fees. Leasing of retail space is projected to generate an additional \$4,800 in business and occupation taxes to the City of Morgantown.

***The building will also generate a total of \$250,000 in real property tax revenues to state and local governmental units of which the City of Morgantown will receive \$51,474.***

***The City of Morgantown is projected to receive a total of nearly \$107,000 in revenues during the first year of operation. A comparable amount will be generated thereafter on an annual basis.***

***Assuming at least a 30-year project lifespan, the building should generate at least \$3 million to the City of Morgantown.***

## Economic Impact Methodology

The economic impact methodology used in this report is provided by the IMPLAN<sup>®</sup> input-output modeling system.<sup>2</sup> This is an internationally recognized modeling software and data system, which has been used in numerous economic impact studies. The economic impacts reported below are based upon the estimated construction and operational expenses associated with this building. The *direct* impacts result from CASL's expenditures within the Monongalia County economy. These expenditures support various suppliers and vendors who in turn employ individuals and purchase goods and services from their suppliers. For example, CASL's purchases electricity from Mon Power during the construction period. This utility has power plants at Fort Martin, which are located within Monongalia County. This plant has employees and purchases goods and services from other suppliers, some of who are also reside within Monongalia County. To the extent the coal used at Fort Martin comes from Monongalia County mines, there are additional economic impacts. The *indirect* impact traces and quantifies all of the backward economic links resulting from CASL's expenditures during the construction phase.

The *induced* impact result from the expenditures in Monongalia County by CASL's employees or contract employees along with those of the employees at businesses supplying the project and, in turn, their suppliers' employees, etc. Examples of these purchases include groceries, medical services, utilities, housing, gasoline, etc. The total economic impact is the sum of the direct, indirect and induced economic impacts estimated using the IMPLAN<sup>®</sup> input-output modeling system.

### Economic Impacts: Construction and Operation

Table 1 presents the economic impacts on the Monongalia County economy associated with the construction phase of the building project (years 2014 and 2015). In the short-run over 200 job years<sup>3</sup> of employment are associated with the construction phase.

Table 1 Economic Impacts of Construction on Monongalia County

	Direct	Indirect	Induced	Total
Employee Compensation (millions 2014\$)	\$8.5	\$1.6	\$1.8	\$11.9
Value Added (millions 2014\$)	\$10.3	\$2.4	\$3.2	\$15.9
Output (millions 2014\$)	\$19.0	\$4.0	\$5.1	\$28.2
Employment (job years)	120	40	43	203

Notes: Rows may not sum due to rounding.

<sup>2</sup> For more information see [www.implan.com](http://www.implan.com).

<sup>3</sup> A job year is one job over one year.

Table 2 presents the economic impacts associated with the first full year of full occupancy leasing of the apartments and retail space. The resulting employment is a combination of full and part-time jobs.

Table 2 Economic Impact of Occupancy on the Monongalia County Economy

	Direct	Indirect	Induced	Total
Employee Compensation (millions 2014\$)	\$0.4	\$0.2	\$0.1	\$0.8
Value Added (millions 2014\$)	\$2.5	\$0.5	\$0.2	\$3.1
Output (millions 2014\$)	\$3.6	\$0.7	\$0.3	\$4.7
Employment (jobs)	31	6	3	40
Notes: Rows may not sum due to rounding.				

These estimates, however, do not include the economic impacts associated with expenditures by 368 tenants in downtown Morgantown. During any year WVU releases estimates of the estimated cost of attendance expected by students seeking financial aid. ***These estimates include nearly \$2,000 per student (and as much as \$3,000+) for miscellaneous living expenses on retail, books and supplies. Thus, in a given year the occupants of the building could add upwards of nearly \$700,000 in spending in necessity retail stores, restaurants, food stores, entertainment, book stores, drug stores, beauty and hair salons, etc., in the downtown area.*** Financial institutions located downtown may also see an increase in financial deposits due to the proximity of these tenants.

## Conclusions

These conclusions are based on the projected construction and operation costs and revenues associated with the plant as provided to Witt Economics LLC. The total economic impact is the sum of the direct, indirect and induced economic impacts estimated using the IMPLAN® input-output modeling system. Changes in the project cost and/or vendor locations may change the estimated economic impacts from those reported herein.

These impacts, however, can be viewed as conservative as they exclude other economic impacts associated with student renters. For example, additional student renters in the Morgantown downtown urban core provides an economic boost to the retail sales and services provided, in large part, to Main Street Morgantown businesses. Locating students close to the downtown campus of West Virginia University permits these students to shift their transportation options from cars and trucks to Mountain Line Transit Authority buses, motorcycles, bicycles and pedestrian modes. With the increasing costs of vehicle ownership and maintenance,

more young adults are seeking residential locations with more transportation alternatives.

One should not ignore the potential enhancement of Morgantown's economic potential that is validated through the significant corporate investment by a national firm in this signature downtown project. While many trumpet the attraction of national retail and restaurant chains to the Morgantown peripheries, this project firmly establishes downtown Morgantown as a good investment.

Finally, West Virginia University President Gordon Gee has expressed his interest in growing the student body to 40,000 students over time.<sup>4</sup> Providing housing for these students close to campus permits accommodation of this growth in a sustainable, cost-effective manner that will encourage further revitalization of Downtown Morgantown, versus having housing on the outskirts of the metro area. These economic impacts assume the absorption of this housing into the housing inventory will be easily accommodated.

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<sup>4</sup> *The Exponent Telegram*, March 30, 2014. Available from [http://www.theet.com/news/local/e-gordon-gee-discusses-wvu-its-role-its-future/article\\_19079ff0-b7b7-11e3-a41c-0019bb2963f4.html](http://www.theet.com/news/local/e-gordon-gee-discusses-wvu-its-role-its-future/article_19079ff0-b7b7-11e3-a41c-0019bb2963f4.html).

## Appendix A: Economic Impact Definitions

Employment:	The number of jobs in a business, industry, or region. Also, the number of jobs attributable to an impact (see below). This is a measure of the number of full-time and part-time positions, not necessarily the number of employed persons. Jobs are annual average by place of work. A job year is equivalent to one job for one year.
Employee Compensation:	Wages and salaries plus employers' contribution for social insurance (social security, unemployment insurance, workers compensation, etc.) and other labor income (pension contributions, health benefits, etc.). By place of work unless otherwise stated.
Impacts:	The results of the recirculation of funds throughout a regional economy due to the activity of a business, industry, or institution. Estimated by tracing back the flow of money through the initial businesses' employees and suppliers, the businesses selling to the employees and suppliers, and so on. Thus, they are a way to examine the distribution of industries and resources covered in the costs of the initial activity.
Output:	For most sectors, measured as sales plus net inventories and the value of intra-corporate shipments. For retail and wholesale trade, measured as gross margins (i.e. sales minus cost of goods sold, also equal to the mark-up on goods sold).
Value Added:	A measure of the value created by a business or industry or attributable to an impact (see above). Equal to the value of production minus the cost of purchased goods and services. Also equal to employee compensation plus capital income (profits, interest paid, depreciation charges), and indirect business taxes (e.g. severance, excise). Corresponds to the aggregate concepts of gross domestic product (GDP).

## **Appendix B: Author Biography**

The author of this report, Tom S. Witt, Ph.D. is the managing director and chief economist, Witt Economics LLC. Prior to this position, Dr. Witt was professor of economics and director, Bureau of Business and Economic Research, West Virginia University, from which he retired in 2012, completing 42 years of service to West Virginia University. The author of numerous research articles and monographs, he also was the principal or co-investigator on over \$6 million in sponsored research at WVU. He has served as a consultant to West Virginia state agencies including the Legislature, Governor's Office, Department of Education, Division of Highways, and Department of Revenue, among others. He has also served as a consultant to Charleston Area Medical Center, Columbia Gas, Advantage Valley, Braskem, West Virginia Wesleyan College, West Virginia School of Osteopathic Medicine, and others.

Dr. Witt received his B.A. degree in economics from Oklahoma State University and his MA and Ph.D. in economics from Washington University (St. Louis). He is a member of the American Economics Association and the National Association for Business Economics.

***Site Plans for subject property were  
provided to Planning Commissioners on  
Tuesday, September 2<sup>nd</sup> 2014.***